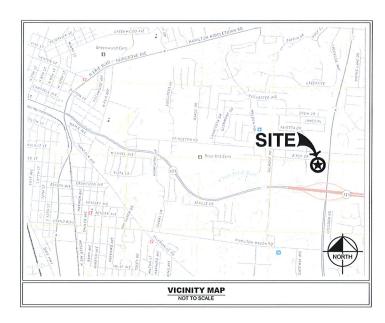
SITE PLAN REVIEW FOR

FLITE BANKING CENTERS, LLC

ADDITION OF ATM DRIVE-UP IN EXISTING WALMART #3502 PARKING LOT 3201 PRINCETON ROAD FAIRFIELD TOWNSHIP, OH

SCHEMATIC DESIGN SET: OCTOBER 26, 2021

(SITE



GENERAL DEVELOPMENT NOTES

- 1. PART OF THE SUBJECT PROPERTY IS LOCATED IN AREAS DESIGNATED AS "ZONE X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2 % ANNUAL CHANCE FLOODPLAIN) AS NOTED ON THE
- CHANCE FLOODPLAIN) AS NOTED ON THE CURRENT FEMA FIRM COMMUNITY PANEL 39017C0194E DATED 12/17/2010.

 2. ANY EXCAVATION, FILL, OR DISTURBANCE OF THE EXISTING GROUND ELEVATION MUST BE DONE IN ACCORDANCE WITH STORM WATER MANAGEMENT ACCURATION OF THE PARTY PROPERTY AND THE CONTROLLED THE PARTY OF THE PARTY PROPERTY AND THE CONTROLLED THE PARTY OF THE PARTY PROPERTY OF THE THE PARTY PARTY OF THE PARTY P ORDINANCE AND APPROVED BY THE FAIRFIELD TOWNSHIP PUBLIC WORKS DEPARTMENT.
- 3. FAIRFIELD TOWNSHIP UTILITY DISTRICT SHALL BE PROVIDED SUFFICIENT AND UNENCUMBERED ACCESS IN ORDER TO MAINTAIN AND REPAIR

ADDITION OF A SINGLE DRIVE-UP ATM LANE IN AN

EXISTING PROPERTY SUMMARY

	FAIRFIELD TOWNSHIP, OH
PLAT:	N/A
LOT:	02
SUBD:	N/A
MAP AND PARCEL ID:	MAP A03, PARCEL 21000039

39017C0194E DATED 12/17/2010 M-1 - LIGHT INDUSTRIAL DISTRICT BUTLER COUNTY VETERANS HWY **LOCATION MAP**

> CITY OF FAIRFIELD TOWNSHIP **BUTLER COUNTY, OH**

PROJECT DESIGN TEAM

CLIENT

FLITE BANKING CENTERS, LLC 8955 KATY FWY, STE 107 HOUSTON, TX 77024 CONTACT: DEANGELO RIOS

CIVIL ENGINEER KIMLEY-HORN AND ASSOCIATES INC. 7965 NORTH HIGH STREET, STE 200 COLUMBUS, OHIO 43235 PHONE: (614) 472-8830 CONTACT: RYAN GRASSLY , P.E

PERMITTING / UTILITY CONTACTS

PHONE: (513) 867-5744

PUBLIC WORKS

8870 N GILMORE ROAD

FAIRFIELD, OH 45014

CONTACT: JEFF BENNET

BUILDING DEPT. BUILDING AND ZONING DIVISION 5350 PLEASANT AVENUE

CONTACT: LELAND ROSATO

ELECTRIC PUBLIC UTILITIES

5021 GROH LANE

TRANSPORTATION DEPT. ZONING BUTLER COUNTY ENGINEER'S OFFICE ZONING DEPARTMENT

PHONE: (513) 785-2266 CONTACT: CHUCK GOINS FIRE CHIEF

FAIRFIELD TOWNSHIP FIRE DEPARTMENT 6032 MORRIS ROAD FAIRFIELD TOWNSHIP, OH 45011 513-887-4400 PHONE: (513) 887-4400 CONTACT: TIMOTHY J. THOMAS SR

SHEET INDEX SHEET NUMBER C0.1 GENERAL NOTES C1.1 C2.0 SITE PLAN C2.1 PIER, PAINTING & STRIPING PLA ATM & BOLLARD PLAN

PROJECT NOTES

- 1. THIS LOCATION USES 9 PARKING SPACES.
 2. ENTIRE ASPHALT WILL BE REMOVED AND REPLACED WITH CONCRETE.
 3. ALL CURBS, INCLUDING ISLAND CURBS, WILL BE PAINTED WHITE.
 4. DIRECTIONAL ARROWS AND "ENTER" & "EXIT" MESSAGING WILL BE PAINTED WHITE.
 5. ALL LIGHT POLE COLUMNS WILL BE FINISHED SMOOTH

- 6. BOLLARD WILL BE PAINTED, HAVE SHALLOW DOME
- CAP, AND SIT 56" ABOVE CONCRETE SURFACE
 7. BOLLARDS WILL BE FILLED WITH CONCRETE,
 EXCEPT BOLLARDS A & B NOTED IN BOLLARD
- DETAIL.

 8. ATM SLAB WILL A MINIMUM 12" THICK TO ALLOW DRILLED ANCHOR POINTS.

 9. LIGHT ACTIVATION FOR LIGHT STANDARDS AND CANOPY WILL BE PERFORMED BY SINGLE PHOTOCELL PLACED ON METER & PANEL STRUCTURE STRUCTURE
- 10. EACH LIGHT POLE WILL RECEIVE ONE 1" DEDICATED DRY CONDUIT TERMINATING AT THE ATM PIT FOR FUTURE CATEGORY 6 CABLE.
- 11. CANOPY EGRESS LEG WILL RECEIVE ONE 2* DEDICATED DRY CONDUIT TERMINATING AT THE ATM PIT FOR FUTURE MULTIPLE CATEGORY 6 12 ATM PIT WILL RECEIVE ONE 2* DEDICATED DRY
- AIM PIT WILL RECEIVE ONE 2 DEBUGATED DRY
 CONDUIT TERMINATE AT METER & PANEL FOR
 COMMUNICATION CABLING

 ATM PIT RECEIVES 2 CIRCUITS (30 AMP FOR ATM &
 20 AMP FOR ANCILLARY DEVICES).

OWNER / DEVELOPER



FLITE BANKING CENTERS, LLC. 8955 KATY FREEWAY, STE. 107 HOUSTON, TX, 77024 PHONE: (281)-816-5391 **CONTACT: DEANGELO RIOS**

PLANS PREPARED BY



7965 North High Street, STE 200, Columbus, OH 43235 Main: (614) 472-8830 | www.kimley-horn.com ©2021 Kimley-Horn and Associates, Inc.

	PLAN REVISIONS				
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CONTRACTOR RESPONSIBILITIES:

- PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL BE RESPONSIBLE FOR:
 - THE CONTRACTOR SHALL VERIFY ALL PROPOSED AND EXISTING CONDITIONS INCLUDING UTILITIES NVERTS, CONNECTIONS, MATERIALS, ETC.) AND DIMENSIONS WITHIN THE LIMITS OF WORK PRIOR TO THE START OF CONSTRUCTION.
- REFER TO ARCHITECTURAL DRAWINGS FOR DETAILED BUILDING INFORMATION.
- THE CONTRACTOR IS RESPONSIBLE FOR ALL NOTIFICATIONS AND LIAISONS WITH UTILITY COMPANIES
- PRIOR TO COMMENCING LAND DISTURBANCE ACTIVITY, THE LIMITS OF LAND DISTURBANCE SHALL BE CLEARLY AND ACCURATELY DEMARCATEO WITH STAKES, RIBBONS, OR OTHER APPROPRIATE MEANS. THE LOCATION AND EXTENT OF ALL AUTHORIZED LAND DISTURBANCE SHALL OCCUR INSIDE THE APPROVED LIMITS INDICATED ON THE APPROVED PLANS.

DURING CONSTRUCTION:

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEVIATIONS FROM THESE PLANS AND
- THE CONTRACTOR SHALL USE MATERIALS AND EMPLOY CONSTRUCTION METHODS IN ORDER TO COMPLY WITH THE DRAWINGS AND SPECIFICATIONS. WHERE A CONFLICT OCCURS, THE STRICTEST DESIGN SHALL GOVERN. THE ENGINEER'S REVIEW OF SHOP DRAWINGS, PRODUCT DATA, ETC., DOES NOT RELIEVE THE CONTRACTOR FROM COMPLYING WITH THE CONTRACT DOCUMENTS. TH CONTRACTOR SHALL INFORM THE ENGINEER IN WRITING OF ANY SPECIFIC DEVIATIONS AND OBTAIN ENGINEER'S WRITTEN APPROVAL OF THE SPECIFIC DEVIATION
- IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO FOLLOW ALL APPLICABLE SAFETY CODES AND REGULATIONS DURING ALL PHASES OF CONSTRUCTION.
- ALL CONSTRUCTION MUST CONFORM TO THE STANDARDS, SPECIFICATIONS, AND CODES OF THE
- CONSTRUCTION SHALL MEET ALL CURRENT STANDARDS SET FORTH IN THE AMERICANS WITH DISABILITIES ACT.
- IF THE CONTRACTOR DAMAGES ANY EXISTING UTILITIES DURING CONSTRUCTION, HE SHALL, AT HIS OWN EXPENSE, REPLACE OR REPAIR THE UTILITIES TO ORIGINAL CONDITION AND QUALITY AS APPROVED BY THE OWNER AND REPRESENTATIVE OF THE APPROPRIATE UTILITY COMPANY.
- SUFFICIENT BARRICADES, LIGHTS, SIGNS, AND OTHER TRAFFIC CONTROL METHODS IN ACCORDANCE WITH GOVERNING ORDINANCES MAY BE NECESSARY FOR THE PROTECTION AND SAFETY OF THE PUBLIC. SAID CONTROL DEVICES SHALL BE PER THE MANUAL OF TRAFFIC CONTROL DEVICES. M.U.T.C.D., CURRENT EDITION, AND SHALL BE PROVIDED AND MAINTAINED THROUGHOUT
- TRAFFIC CONTROLS AND OTHER WARNING DEVICES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF ANY WORK ON CITY, COUNTY, OR OHIO DEPARTMENT OF TRANSPORTATION ROADS. THEY SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION AND SHALL REMAIN IN PLACE UNTIL THE CONCLUSION OF ALL WORK.
- ALL WARNING DEVICES SHALL BE EITHER TYPE I BARRICADES OR DRUMS WITH WARNING LIGHTS ON EVERY OTHER DEVICE. THEY SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITION, AND FAIRFIELD TOWNSHIP STANDARDS FOR COLOR, SIZE, REFLECTIVITY, HEIGHT, AND PLACEMENT
- FIRE DEPARTMENT ACCESS SHALL BE MAINTAINED AT ALL TIMES.
- CONTRACTOR SHALL SHORE AND BRACE ALL EARTH, FORMS, CONCRETE, STEEL, WOOD, AND MASONRY TO RESIST GRAVITY, EARTH, WIND, THERMAL, CONSTRUCTION, AND MISCELLANEOUS LOADS DURING CONSTRUCTION.
- I. ON-SITE BURIAL OF DEBRIS IS PROHIBITED.
- M. UNLESS OTHERWISE NOTED THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OF ALL FABRICATED TERIALS TO THE ENGINEER. DESIGN DOCUMENTS SHALL NOT BE REPRODUCED AS SHOP
- N. IN CASE OF UNFORESEEN CONSTRUCTION COMPLICATIONS OR DISCREPANCIES, THE CONTRACTOR IS TO IMMEDIATELY NOTIFY THE ENGINEER IN WRITING
- O. ALL REQUIRED TESTING REPORTS SHALL BE AVAILABLE AT THE JOB SITE.
- AS-BUILT DRAWINGS OF ROADWAYS, STORM DRAINS, SANITARY SEWER AND WATER LINES, FIELD APPROVAL BY THE ENGINEER, AND ALL APPLICABLE BONDS ARE REQUIRED PRIOR TO FIN. ACCEPTANCE BY THE OWNER.
- Q. CONTRACTOR SHALL MAINTAIN CONTINUOUS UTILITY SERVICE TO ALL EXISTING BUILDINGS THROUGHOUT CONSTRUCTION UNLESS APPROVAL FOR SERVICE INTERRUPTION IS OBTAINED FROM
- THE CONTRACTOR SHALL VERIFY EXISTING CONDITIONS TO ENSURE THAT THE NEW WORK SHALL FIT INTO THE EXISTING SITE IN THE MANNER INTENDED AND AS SHOWN ON THE DRAWINGS. SHOULD ANY CONDITIONS EXIST THAT ARE CONTRARY TO THOSE SHOWN ON THE DRAWINGS. THE CONTRACTOR CONDITIONS EXIST THAT ARE CONTRARY TO THOSE SHOWN ON THE DRAWINGS, THE CONTRAC SHALL NOTIFY THE OWNER'S REPRESENTATIVE PRIOR TO PERFORMING ANY WORK IN THE AREA INVOLVING DIFFERENCES. NOTIFICATION SHALL BE IN THE FORM OF A DRAWING OR SKETCH INDICATING FIELD MEASUREMENTS AND NOTES RELATING TO THE AREA.
- ANY FOREIGN ITEM FOUND DURING CONSTRUCTION IS THE PROPERTY OF THE OWNER. THIS INCLUDES, BUT IS NOT LIMITED TO, PRECIOUS METALS, COINS, PAPER CURRENCY, ARTIFACTS AND
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE DONE TO THE PREMISES OR ADJACENT PREMISES OR INJURIES TO THE PUBLIC DURING THE CONSTRUCTION OF THE WORK WHETHE CAUSED BY HIMSELF, HIS SUBCONTRACTORS, OR THE CARELESSNESS OF ANY OF HIS EMPLOYEES.
- THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN ALL NECESSARY TEMPORARY WORKS FOR THE PROTECTION OF THE WORK AND THE PUBLIC, INCLUDING BARRICADES, WARNING SIGNS, LIGHTS,
- THE CONTRACTOR ACKNOWLEDGES & AGREES THAT THE WORK IS ENTIRELY AT HIS RISK UNTIL SITE IS ACCEPTED, AND HE WILL BE HELD RESPONSIBLE FOR ITS SAFETY BY THE OWNER. THE CONTRACTOR WILL INDEMNIFY THE OWNER & OWNER'S REPRESENTATIVE FROM LIABILITY AT THE SITE THROUGHOUT THE CONSTRUCTION PROCESS
- THE CONTRACTOR SHALL GIVE ALL NECESSARY NOTICES AND OBTAIN ALL PERMITS AND PAY ALL LEGAL FEES. HE SHALL ALSO COMPLY WITH ALL CITY, COUNTY AND STATE BUILDING LAWS, ORDINANCES OR REGULATIONS RELATING TO BUILDING SIDEWALKS, STREETS, BLASTING, PUBLIC INFRASTRUCTURE, STORMWATER REGULATIONS, ETC.
- THE CONTRACTOR IS TO CHECK AND VERIFY ALL MEASUREMENTS, LEVELS, ETC. BEFORE ORDERING MATERIALS AND PROCEEDING WITH THE WORK, AND IS TO BE RESPONSIBLE FOR THE SAME.
- REFERENCE POINTS AND HUBS DURING THE CONSTRUCTION OF HIS WORK, AND SHALL BEAR THE
- CARE SHALL BE TAKEN TO PROTECT ANY UTILITIES, TREES, ETC. WHICH ARE TO REMAIN AND NOT OBE DISTURBED BY THE CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGES TO SUCH PROPERTY.

DEMOLITION INFORMATION:

NOTIFICATIONS:

THE CONTRACTOR SHALL NOTIFY THE OWNER AND CITY INSPECTOR(S) 24 HOURS PRIOR TO ANY DEMOLITION OR CONSTRUCTION.

- ONLY ITEMS SPECIFICALLY NOTED TO BE DEMOLISHED SHALL BE REMOVED FROM THE SITE
- REMOVE EXISTING PAVED AREAS AS SHOWN INCLUDING DRIVEWAYS, SIDEWALKS, PARKING AREAS, SERVICE AREAS, EQUIPMENT PADS, AND ALL MISCELLANEOUS PAVING
- ALL DEBRIS RESULTING FROM DEMOLITION SHALL BE REMOVED FROM THE SITE OF LEGALLY BY THE CONTRACTOR IN ACCORDANCE WITH LOCAL STATE AND FEDERAL REGULATIONS. BACKFILL ALL TRENCHES AND EXCAVATIONS RESULTING FROM DEMOLITION.
- D. ALL DEMOLISHED MATERIAL BECOMES THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE

TREE PROTECTION GUIDELINES:

PROTECT ALL EXISTING TREES NOTED "TO REMAIN" AND ALL ITEMS TO BE TURNED OVER TO THE OWNER DURING DEMOLITION. TAKE ALL NECESSARY PRECAUTIONS AND PROTECTIVE MEASURES. ANY EXISTING ITEMS TO BE TURNED OVER TO THE OWNER WHICH ARE DAMAGED DURING DEMOLITION SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER. TREES WHICH ARE DAMAGED WILL BE REPLACED OR REIMBURSED AT A RATE TO BE DETERMINED BY THE OWNER

- PRIOR TO REMOVING OR ABANDONING ANY UTILITY THE CONTRACTOR SHALL VERIFY THAT NO UPSTREAM SERVICE WILL BE TERMINATED. THE CONTRACTOR SHALL INFORM THE ENGINEER IN WRITING OF ANY TERMINATION NOT SHOWN ON THE PLANS.
- ALL ABANDONED WATER LINES, STORM SEWER PIPE, SANITARY SEWER PIPES, GAS LINES, OR ANY OTHER ABANDONED UNDERGROUND UTILITY SHALL BE ABANDONED IN PLACE UNLESS NOTED

SITE INFORMATION:

- THE FOLLOWING ARE APPLICABLE TO ALL CIVIL DOCUMENTS:
 - WHERE A DETAIL SECTION, TYPICAL SECTION, OR A NOTE IS SHOWN FOR ONE CONDITION. IT SHALL APPLY FOR ALL LIKE OR SIMILAR CONDITIONS, UNLESS OTHERWISE NOTED ON THE PLANS
- B. EXISTING AND PROPOSED CONTOURS ARE AT ONE (1) FOOT INTERVALS.
- ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED
- ALL PIPE LENGTHS SPECIFIED IN THESE PLANS ARE THE HORIZONTAL DISTANCE AND ARE SHOWN FOR REFERENE ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE ACTUAL LENGTHS BASED ON PROPOSED PIPE SLOPE.
- PIPE LENGTHS IN PLANS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE

EROSION AND SEDIMENT CONTROL INFORMATION:

- - THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO OR CONCURRENT WITH LAND-DISTURBING ACTIVITIES.
 - PROVISIONS TO PREVENT EROSION OF SOIL FROM THE SITE SHALL BE AT A MINIMUM IN CONFORMANCE WITH THE REQUIREMENTS OF THE OHIO EROSION AND SEDIMENT CONTROL.

 HANDBOOKS. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE
 EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL

 MERCHANDER OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE
 EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE
 IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
 - FAILURE TO INSTALL, OPERATE, OR MAINTAIN ALL EROSION CONTROL MEASURES WILL RESULT IN ALL CONSTRUCTION BEING STOPPED ON THE JOB SITE UNTIL SUCH MEASURES ARE CORRECTED BACK TO THE STANDARDS SPECIFIED IN THE OHIO EROSION AND SEDIMENT CONTROL HANDBOOKS, CURRENT
 - EROSION CONTROL MEASURES WILL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THI APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT
 - EROSION CONTROL DEVICES SHALL BE INSTALLED PRIOR TO LAND DISTURBANCE. THE LOCATION OF SOME OF THE EROSION CONTROL DEVICES MAY HAVE TO BE ALTERED FROM THAT SHOWN ON THE APPROVED PLANS IF DRAINAGE PATTERNS DURING CONSTRUCTION ARE DIFFERENT FROM THE FINAL PROPOSED DRAINAGE PATTERNS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ACCOMPLISH EROSION CONTROL FOR ALL DRAINAGE PATTERNS CREATED AT VARIOUS STAGES DURING CONSTRUCTION. ANY DIFFICULTY IN CONTROLLING EROSION DURING ANY PHASE OF CONSTRUCTION SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY
 - THE CONSTRUCTION OF THE SITE WILL COMMENCE WITH THE INSTALLATION OF EROSION CONTROL MEASURES SUFFICIENT TO CONTROL SEDIMENT DEPOSITS AND EROSION. ALL SEDIMENT CONTROL WILL BE MAINTAINED UNTIL ALL UPSTREAM GROUND WITHIN THE CONSTRUCTION AREA HAS BEEN COMPLETELY STABILIZED WITH PERMANENT VEGETATION AND ALL ROADS/DRIVEWAYS HAVE BEEN
 - CONSTRUCTION EXITS SHALL BE CONSTRUCTED AT EACH POINT OF ENTRY OR EXIT FROM THE SITE CONSTRUCTION EXITS SHALL BE CONSTRUCTED AT EACH POINT OF ENTRY OR EXIT FROM THE SITE AND SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MIJD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH STONE AS CONDITIONS DEMAND, REPAIR, AND/OR CLEANOUT OF ANY STRUCTURES USED TO TRAP SEDIMENT, ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES OFF SITE ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY. ACCESS POINTS PROTECTED WITH A CONSTRUCTION EXIT SHALL BE OTHERWISE BARRICADED UNTIL THE SITE IS STABILIZED.

ADA COMPLIANCE:

- CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED BASED ON THE CITY STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS.
- PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WARNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.
- ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND AVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION
- ANY COMPONENTS OF THE PROJECT SERVING MULTIFAMILY DWELLINGS IN BUILDINGS THAT HAVE 4 OR MORE UNITS PER DWELLING SHALL ALSO CONFORM TO THE FAIR HOUSING ACT (FHA), AND COMPLY WITH THE FAIR HOUSING ACT DESIGN MANUAL BY THE US DEPARTMENT OF HOUSING AND DUBBAN DEVELOPMENT.
- BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESS AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL IN NO CASE SHALL SIDEWALK CROSS SLOPE EXCEED 2.0 PERCENT, IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT, ACCESSIBLE PA SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.
- CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.

CONCRETE INFORMATION (SITE WORK ONLY):

- PRODUCT CRITERIA:
 - UNLESS OTHERWISE NOTED CEMENT SHALL BE TYPE I OR III CONFORMING TO ASTM C150. AGGREGATES SHALL BE NORMAL WEIGHT CONFORMING TO ASTM C33.
 - CONCRETE SHALL CONFORM TO ACI BUILDING CODE (318-89). UNLESS NOTED CONCRETE SHALL BE NORMAL WEIGHT AND HAVE A 28 DAY COMPRESSIVE STRENGTH OF 3,500 P.S.I.
 - C. SLUMP SHALL BE 3* TO 5* FOR REGULAR MIX. LARGER SLUMP SHALL BE PERMITTED WITH WATER REDUCING ADMIXTURES AND WRITTEN CONSENT OF THE ENGINEER.
 - THE RELATIONSHIP BETWEEN MAXIMUM AGGREGATE SIZE TO MINIMUM AMOUNT OF CEMENT IN CONCRETE PAVEMENT (IB. PER C.Y. OR MIX) SHALL BE AS FOLLOWS: 1* 520, 34* 540, 12* 590, 36* 610.
- 2. CURING CRITERIA:
 - CONCRETE CURING SHALL COMPLY WITH ACI 308. CURING PROCESS SHALL START IMMEDIATELY FOLLOWING INITIAL SET. CURING SHALL BE BY CURING CO
 - CONCRETE EXPOSED TO THE WEATHER SHALL BE AIR-ENTRAINED IN ACCORDANCE WITH ACI 318-89 TABLE 4.1.1. NORMAL WEIGHT CONCRETE SLABS SHALL HAVE AIR CONTENT IN ACCORDANCE WITH ACI 302.IR-88 TABLE 5.2.7A.
 - HOT WEATHER CONCRETING SHALL COMPLY WITH ACI 305. NO CONCRETE ABOVE 90 DEGREES FAHRENHEIT SHALL BE POURED. LOWER CONCRETE TEMPERATURE BY COOLING WATER AND AGGREGATE. FORMS, STEEL AND SURGRADE SHALL RE SPRINKLED WITH COLD WATER. AFTER FINISHING CONCRETE USE LIGHT FOG SPRAY UNTIL CURING COMPOUND IS USED.
 - COLD WEATHER CONCRETING SHALL COMPLY WITH ACI 306. SPECIAL MATERIAL PROCEDURES SHALL BE PROVIDED DURING PLACING AND CURING OF CONCRETE BELOW 40 DEGREES FAHRENHEIT.
 - CURING, HOT, AND COLD WEATHER CONCRETING PROCEDURES ARE ONLY GIVEN AS A GUIDE, IT IS THE CONTRACTOR'S RESPONSIBILITY TO PREVENT CONCRETE DAMAGE AND CRACKS. DAMAGED OR CRACKED CONCRETE WILL NOT BE ACCEPTED.

REINFORCING STEEL INFORMATION (SITE WORK ONLY):

- PRODUCT CRITERIA:
- A. REINFORCING STEEL SHALL CONFORM TO ASTM A-615, GRADE 60 AND GRADE 40 FOR #3 AND SMALLER BARS. MINIMUM LAP 48" DIAMETE
- WELDED WIRE FABRIC SHALL CONFORM TO ASTM A-185, MINIMUM LAP SHALL BE 81.
- C. REINFORCING THAT IS WELDED SHALL BE WELDABLE TYPE AND CONFORM TO ASTM

PAVEMENT INFORMATION:

- PAVEMENT:
 - A. ALL MATERIALS, EQUIPMENT, METHODS OF CONSTRUCTION, AND WORKMANSHIP SHALL CONFORM TO THE OHIO DEPARTMENT OF TRANSPORTATION, ODOT, STANDARI SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION.
 - SEE PAVEMENT DETAILS ON CONSTRUCTION DOCUMENTS FOR SPECIFIC DESIGN
- ALL CURB AND GUTTER TO BE 24" AND CONSTRUCTED OF 3000 P.S.I. CONCRETE UNLESS OTHERWISE NOTED.
- 2. SIGNING AND STRIPING:
 - SIGNING AND STRIPING TO BE PROVIDED BY THE CONTRACTOR ACCORDING TO THE
- ALL PAVEMENT MARKINGS SHALL CONFORM TO CURRENT MUTCD STANDARDS, ALL PAVEMENT MARKINGS ON PRIVATE PROPERTY SHALL BE PAINT, UNLESS NOTED OTHERWISE. ALL PAVEMENT MARKINGS ON PUBLIC RIGHT-OF-WAY SHALL BE THERMOPLASTIC, UNLESS NOTED OTHERWISE.

STORM SEWER NOTES:

- REINFORCED CONCRETE PIPE SHALL BE RCP CLASS III UNLESS OTHERWISE NOTED WITH BELL-AND-SPIGOT AND GASKETED JOINTS WITH ASTM C 443 RUBBER GASKETS.
- 2. FILL HEIGHTS OVER 13' REQUIRE CLASS IV RCP STORM PIPE.
- 3. STORM INLETS SHALL BE PRECAST IN ACCORDANCE WITH FAIRFIELD TOWNSHIP PUBLIC WORKS SPECIFICATIONS WHICH MEET OR EXCEED ASTM C 478.
- 4. ALL MANHOLE FRAMES AND COVERS ARE TO BE PER FAIRFIELD TOWNSHIP PUBLIC WORKS ACL INVANCE PROMISES AND COVERS ARE 10 BE PERF PAIR IELD TOWNSHIP PUBLIC WORKS STANDARD DETAIL FOR DIMENSIONS AND MATERIALS AND AS BELOW IF NOT OTHERWISE INDICATED. FERROUS; 24-INCH (610-MM) ID BY 7-TO 9-INCH (175-TO 225-MM) RISER WITH 4-INCH-(102-MM-) HINMIMUM WIDTH FLANGE AND 26-INCH- (660-MM-) DIAMETER COVER, INCLUDE INDENTED TOP DESIGN WITH LETTERING CAST INTO COVER, USING WORDING EQUIVALENT TO COVER, USING WORDING EQUIVALENT TO
- 5. MATERIAL: GRAY IRON ASTM A48 CLASS 30 UNLESS OTHERWISE INDICATED.
- 6. ALL HDPE PIPE SHALL BE CORRUGATED HIGH DENSITY POLYETHYLENE SMOOTH INTERIOR PIPE. HOPE PIPE SHALL CONFORM TO ASTM D3350 WITH SOIL TIGHT JOINTS
- ALL HDPE SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS AND AASHTO
- 8. CONTRACTOR TO PROVIDE AND INSTALL MANUFACTURER RECOMMENDED FITTINGS ON RCP CONNECTIONS TO HDPE STRUCTURES.
- 9. REFER TO PIPE CHART FOR CASTING TYPES, INSTALL REDUCERS AS NECESSARY PER MANUFACTURER'S SPECIFICATIONS TO ACCOMMODATE LARGER INLET SIZES
- 10. FOLLOW CONSTRUCTION PLANS AND MANUFACTURER DETAILS, SPECIFICATIONS, AND INSTALLATION INSTRUCTION AS INCLUDED WITHIN THE PLANS AND PROVIDED BY
 MANUFACTURER FOR THE INSTALLATION OF WATER QUALITY AND DETENTION SYSTEMS
- CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR ALL STORM SEWER PIPE, STRUCTURES, WATER QUALITY STRUCTURES, AND DETENTION STRUCTURES FOR ENGINEER AND OWNER APPROVAL PRIOR TO GODERING MATERIALS.

CLEARING AND GRUBBING:

- A. DO NOT EXCEED CLEARING AND GRUBBING LIMITS OF CONSTRUCTION LINES INDICATED
- ALL AREAS OUTSIDE THE LIMITS OF CONSTRUCTION LINE SHALL NOT BE CROSSED BY HEAVY EQUIPMENT OR USED FOR STORING HEAVY EQUIPMENT OR MATERIALS
- C. NO EQUIPMENT SHALL BE STORED UNDER THE DRIP LINE OF TREES TO REMAIN.
- D. DO NOT FALL ANY TREES OR PUSH PILES OF DEBRIS AGAINST ANY TREES TO REMAIN
- REMOVE ALL STUMPS, ROCKS, ASPHALT & CONCRETE DEBRIS, ETC. AND DISPOSE OFF SITE IN ACCORDANCE WITH LOCAL, STATE & FEDERAL REGULATIONS.
- CONTACT ALL UTILITY AUTHORITIES WHO HAVE LINES WITHIN THE CLEARING AND GRUBBING LIMITS BEFORE STARTING WORK
- ALL EROSION CONTROL SEDIMENT BARRIERS, SILT FENCES, AND TREE PROTECTION DEVICES SHALL BE INSTALLED PRIOR TO STARTING CLEARING AND GRUBBING.
- AFTER STAKING IS COMPLETED, TREES WITHIN GRADING LIMITS TO BE SAVED WILL BE IDENTIFIED BY THE OWNER'S REPRESENTATIVE. FIELD CHANGES TO GRADING PLANS SHALL BE MADE FOR SMOOTH TRANSITION OF GRADES AROUND ALL TREES WHICH REQUITE TREE WELLS WITHIN THE GRADING LIMITS.
- G. ALL CLEARING SHALL BE LIMITED TO AREAS TO BE GRADED WITHIN 15 CALENDAR DAYS

GRADING NOTES:

- A. TOPSOIL SHALL BE STORED ON SITE IN LOCATIONS APPROVED BY THE OWNER'S REPRESENTATIVE, DRAINAGE SHALL ROUT AROUND THESE TOPSO!! STOCKP!! ES FOR THE DURATION OF THE GRADING OPERATIONS. EROSION CONTROL MEASURES SHALL
- UNSUITABLE SOILS SHALL BE UNIFORMLY SPREAD ACROSS NON-STRUCTURAL FILL AREAS AND COVERED WITH TOPSOIL AND SEEDED.
- WEIGHT OF FIFTEEN TONS PRIOR TO BEGINNING FILL OPERATION, AREAS WHICH ARE SOFT OR UNSTABLE SHALL BE UNDERCUT UNTIL STABLE SOILS ARE FOUND RE-COMPACTION OF THESE SOILS SHALL BE TO 98% MAXIMUM DRY DENSITY AS PER ASTM D698 (STANDARD PROCTOR).
- E. ALL GRADING SHALL BE COMPLETED TO THE LEVEL INDICATED BY THE SCOPE OF WORK LISTED IN THE BID DOCUMENTS
- GRADING SHALL BE SEQUENCED SO THAT BASE STONE IS PLACED WITHIN 10 CALENDAR DAYS PF ACHIEVING OPTIMUM SUBGRADE COMPACTION.
- QUALIFIED SOILS TESTING LABORATORY/ ENGINEER TO OBSERVE THIS WORK AND MAKE TESTS AS REQUIRED.
- AND APPROVED BY DESIGNATED TESTING LABORATORY BEFORE MOVING IT TO THE JOB
- ADEQUACY IN THOSE AREAS, IF SOILS ARE NOT ADEQUATE TO BEAR WEIGHTS THAT WILL BE IMPOSED, TESTING LABORATORY WILL OBSERVE AND REPORT CORRECTIVE ACTION TAKEN
- TEST IN-PLACE SOIL AND FILLED AND COMPACTED AREAS. IF THESE ARE NOT ADEQUATE TO BEAR WEIGHTS IMPOSED, TESTING LABORATORY WILL ADVICE THE OWNER'S REPRESENTATIVE OF THEIR RECOMMENDATIONS. HE WILL DIRECT ANY CORRECTIVE
- THE SOILS ENGINEER'S AND TESTING LABORATORY'S FEES WILL BE PAID BY THE
- APPLICABLE SPECIFICATIONS FOR COMPACTED FILL: THE FOLLOWING CURRENT AMERICAN SOCIETY OF TESTING MATERIALS (ASTM) STANDARDS ARE HEREBY MADE PART OF THIS SPECIFICATION:
- D421-58, DRY PREPARATION OF SOIL SAMPLES FOR GRAIN-SIZE ANALYSIS AND
- DETERMINATION OF SOIL CONSTANTS D422-63, STANDARD METHOD OF PARTICLE SIZE ANALYSIS OF SOLLS
- sieve.

 De88, METHOD FOR LABORATORY COMPACTION CHARACTERISTICS OF SOIL USING STANDARD EFFORT

 D1557-78, STANDARD TEST METHODS FOR MOISTURE-DENSITY RELATIONS OF SOILS AND SOIL-AGGREGATE MIXTURES USING 10 LB. (4.54-KG) RAMMER AND 18-INCH (457
- PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDINGS AT ALL LOCATIONS UNLESS OTHERWISE NOTED
- CONTRACTOR SHALL REVIEW THE SITE SPECIFIC GEOTECHNICAL REPORT PRIOR TO COMMENCING WITH GRADING OPERATIONS, WHERE CONFLICTS BETWEEN THE GRADING NOTES AND GEOTECHNICAL REPORT EXIST, THE MORE STRINGENT REQUIREMENT
- Q. CONTRACTOR IS RESPONSIBLE FOR DOCUMENTING WITH LOAD TICKETS, PHOTOGRAPHS, LOG BOOK, VIDEO RECORDING AND OTHER MEANS AS NECESSARY TO VERIFY THE INSTALLATION OF STORMWATER BEST MANAGEMENT PRACTICES REQUIRE BY THE LOCAL MUNICIPALITY AND JURISDICTION, AS-BUILT TOPOGRAPHY AND UTILITY

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GENERAL NOTES

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PREVENT THE LOSS OF TOPSOIL MATERIAL

FILL AREA SHALL BE PROOF-ROLLED WITH RUBBER-TIRED EQUIPMENT WITH A MINIMUM

- CUT AREA SHALL BE PROOF-ROLLED AFTER FINAL SUBGRADE IS ACHIEVED IN THE SAME MANNER AS FILLED AREAS. SOFT OR UNSTABLE SOILS SHALL BE SCARIFIED TO A DEPTH OF 12" AND RE-COMPACTED TO 98% MAXIMUM DRY DENSITY AS PER ASTM D698
- F. ELEVATIONS SHOWN ON THE PLANS IS THE FINISH GRADE ELEVATION.
- SOILS TESTING LABORATORY/ SOILS ENGINEER, CONTRACTOR WILL EMPLOY A
- HAVE EARTH BORROW FILL, AGGREGATE, AND TOPSOIL, AND STRUCTURAL FILL TESTED
- CONTRACTOR SHALL OBSERVE PROOF-ROLLING OF AREAS WHERE BUILDING AND PAVING WILL BE LOCATED TO DETERMINE ADEQUACY OF SOIL COMPACTION AND IN-PLACE SOILS. OTHER AREAS WILL BE INSPECTED BY SOILS ENGINEER TO DETERMINE

- D1140-54, METHOD OF TEST FOR AMOUNT OF MATERIAL IN SOILS FINER THAN NO.200

- BY THE LOCAL MUNICIPALITY AND JURISDICTION, AS-BUILT TOPOGRAPHY AND UTILITY PLANS HAVING BEEN PREPARED BY A QUALIFIED LAND SURVEYOR ARE REQUIRED TO BE SUBMITTED TO KIMLEY-HORN AT THE CONCLUSION OF THE PROJECT FOR VERIFICATION OF DESIGN INTENT. ANY MODIFICATIONS TO THE GRADING AND UTILITY SYSTEMS REQUIRED, NOT PREVIOUSLY APPROVED BY THE OWNER AND ENGINEER ARE THE RESPONSIBILITY OF THE CONTRACTOR.

I DEWI 1842/0007 - WAI 3502 Hamilton M-CADIN Plan Sheets CO 1 GENERAL NOTES dup. CO 1 GENERAL NOTES, Oct 27, 2021, 2:390m, by Nick Mark

PRINCETON RD MURPHY OIL USA INC 3219 PRINCETON ROAD FAIRFIELD TOWNSHIP, OI PARCEL: A0300021000040 ARC CAFEUSA001 LLC 3213 PRINCETON ROAD FAIRFIELD TOWNSHIP, OH 3181 PRINCETON ROAD FAIRFIELD TOWNSHIP, OH PARCEL: A0300021000021 WALMART REAL ESTATE BUSINESS TRUST 3201 PRINCETON ROAD FAIRFIELD TOWNSHIP, OH

DISCLAIMER

THESE DRAWINGS WERE PRODUCED WITHOUT THE BENEFIT OF A CURRENT LAND SURVEY. ALL PROPERTY LINES, EASEMENTS, SETBACKS, EXISTING INFRASTRUCTURE AND TITLE DOCUMENTS SHALL BE VERIFIED PRIOR TO START OF CONSTRUCTION. KIMLEY-HORN AND FLITE BANKING CENTERS DO NOT GUARANTEE THE ACCURACY OF SAID PROPERTY LINES, EASEMENTS, SETBACKS, EXISTING INFRASTRUCTURE AND TITLE DOCUMENTS.

CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL FIELD CONDITIONS AND IS TO ALERT THE ENGINEER AND FLITE BANKING CENTERS OF ANY DISCREPANCIES PRIOR TO STARTING CONSTRUCTION. CONTRACTOR TO COORDINATE WITH FLITE BANKING CENTERS PM FOR ALL FINAL PLACEMENTS OF INFRASTRUCTURE.

EXISTING PROPERTY SUMMARY

3201 PRINCETON ROAD FAIRFIELD TOWNSHIP, OH PROPERTY ADDRESS:

PLAT: LOT: SUBD: MAP AND PARCEL ID:

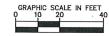
MAP A03, PARCEL 21000039

U.S. FEMA FIRM PANEL: 39017C0194E DATED 12/17/2010

EXISTING ZONING: EXISTING LAND USE:

M-1 - LIGHT INDUSTRIAL DISTRICT COMMERCIAL

ENLARGED VIEW





WALMART #3502 ATM ADDITION FLITE BANKING CENTERS, LLC

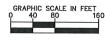
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10/26/2021

SITE LOCATION

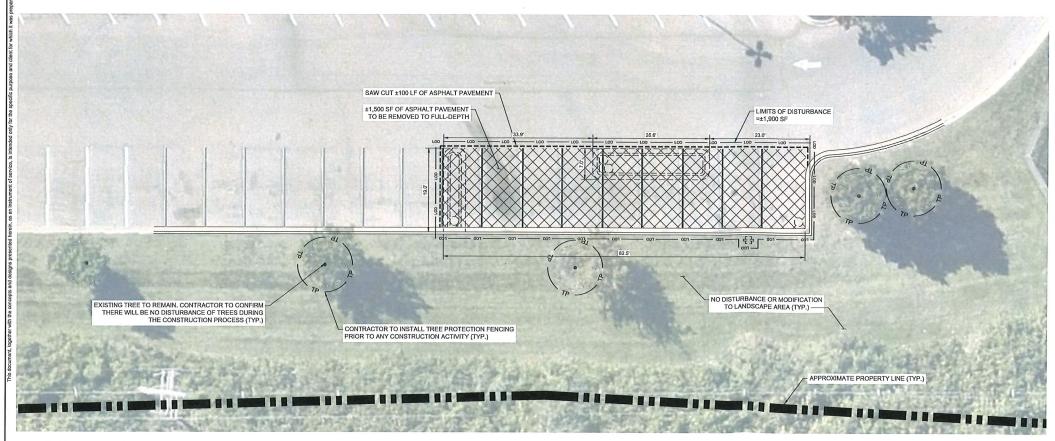
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OVERALL VIEW

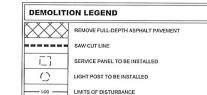




EXISTING CONDITIONS PLAN



DEMOLITION PLAN



EXISTING UTILITIES NOTE

CONTRACTOR SHALL VERIFY LOCATION OF ALL EXISTING UTILITIES AND NOTIFY ENGINEER IMMEDIATEL OF ANY DISCREPANCIES AND/OR CONFLICTS WITH EXISTING OR PROPOSED UTILITIES PRIOR TO PROCEEDING.

- CONTRACTOR TO PROVIDE A 5-0° CHAIN LINK FENCE AROUND CONSTRUCTION AREA.
 CONTRACTOR TO MATCH ALL EXISTING PAYEMENT ELEVATIONS, ALL PROPOSED ISLAND TO BE 6° ABOVE EXISTING PAYEMENT, NEW WORKT O BE DOILE WITHIN THE CONSTRUCTION AREA.
 ONE CALL NOTIFICATION SYSTEM, CALL BEFORE YOU DIGIT
 CONTRACTOR TO SAW CUT, REMOVE AND HAUL OFF EXISTING ASPHALT AND/OR CONCRETE WITHIN DEMO AREA.

DEMOLITION NOTES

- 1. THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSAL (IN A LOCATION APPROVED BY ALL GOVERNING AUTHORITIES) ALL STRUCTURES, PADS, WALLS, FLUMES, FOUNDATIONS, PARRING, DRIVES, DRAINAGE STRUCTURES, UTILITIES, ETC., SUCH THAT THE IMPROVEMENTS SHOWN ON THE PROPOSED PLANS CAN BE CONSTRUCTED. ALL FACILITIES TO BE REMOVED SHALL BE UNDERCUT TO SUITABLE MATERIAL AND BROUGHT TO GRADE WITH SUITABLE COMPACTED FILL MATERIAL PER THE SPECIFICATIONS, PRIOR TO DEMOLITION COCURRING, ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED.

 7. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL DEBRIS FROM THE SITE AND DISPOSING THE DEBRIS IN A LAWFUL MANNER. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL.

 8. CONTRACTOR AUST PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, SHOULD SHOW THE STRUCTURE OF THE CONTRACTOR IS RESPONSIBLE FOR THE STRUCTURES OF ACCURATED THE PUBLIC AT A LL. TIMES WITH FENCING, BARRICADES, ENCLOSURES, SHOULD S

DISCLAIMER

Know what's **below**.

Call before you dig.

THESE DRAWINGS WERE PRODUCED WITHOUT THE BENEFIT OF A CURRENT LAND SURVEY, ALL PROPERTY LINES, EASEMENTS, SETBACKS, EXISTING INFRASTRUCTURE AND TITLE DOCUMENTS SHALL BE VERHIED PRIOR TO START OF CONSTRUCTION. KIMLEY-HORN AND FLITE BANKING CENTERS DO NOT GUARANTEE THE ACCURACY OF SAID PROPERTY LINES, EASEMENTS, SETBACKS, EXISTING INFRASTRUCTURE AND TITLE DOCUMENTS.

CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL FIELD CONDITIONS AND IS TO ALERT THE ENGINEER AND FLITE BANKING CENTERS OF ANY DISCREPANCIES PRIOR TO STATING CONSTRUCTION. CONTRACTOR TO COORDINATE WITH FLITE BANKING CENTERS PM FOR ALL FINAL PLACEMENTS OF INFRASTRUCTURE.

SEE DEMOLITION NOTES, SHEET CO-01.





EXISTING CONDITIONS & DEMOLITION PLAN

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KIMLEY-HORN PROJECT NO.

RBG

SWING ON RELOCATE A UTILITY, THE UTILITY COMPANY OR DEPARTMENT AFFECTED SHALL BE CONTRACTED BY THE CONTRACTOR AND THERE PREMISSION OF STAINED REGRANDING THE METHOD TO CONTRACT BY THE CONTRACTOR SHALL HAVE AVAILABLE AT THE JOB SITE AT ALL TIMES ONE COPY OF THE CONTRACT DOCUMENTS INCLUDING PLANS, SPECIFICATIONS, COPIES OF ANY REQUIRED CONSTRUCTION PERMITS, AND EROSION CONTROL PLANS AND INSPECTION REPORTS (SWPPP).

ANY DISCREPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNERPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNERPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNERPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNER AND ENGINEER OF SHALL SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNERPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT FOR THE ATTENTION OF THE OWNER AND ENGINEER OF SHALL SHALL BE AND AND THE CONTROL OF THE CANOPY OF ANY THE TO BE SAYED SHALL CLOSELY COORDINATED WITH THE CHAPTER OS HAS SHANDARD FOR EXCANATION AND TRENCHING PROCESS FOR MY STREET WORK.

ALL DEMOLITORN WORK OR CONSTRUCTION OF HIGLE TRAFFIC WITHIN 10 FEET OF THE CANOPY OF ANY THEE TO BE SAYED SHALL CLOSELY COORDINATED WITH THE LATEST OSHS STANDARDS FOR EXCANATION AND TRENCHING PRY OF CHIEF EXTENT WITH THE LATEST OSHS STANDARDS FOR EXCANATION AND TRENCHING PRY OF THESE OF PREPARATION SHALL COMPLY FOR THE SECOND FOR THE OWNER AND THE LOCAL MUNICIPALITIES. INTERRUPTION SHALL ONLY OCCUP A PREPARABLE AND THE LOCAL MUNICIPALITIES. INTERRUPTION SHALL DEN HAVE BEEN PROVIDED IN WITHIN SHALL SHAND BEEN AND THE OWNER AND THE LOCAL MUNICIPALITIES. INTERRUPTION SHALL ONLY OCCUP A PAY THE EXCENTION HAS THE AREA PREPARATION OF ANY THE ENTER SHOWN HAVE BEEN AND THE LOCAL MUNICIPALITIES. INTERRUPTION SHALL ONLY OCCUP A PAY THE EXCENTION HAS SHALL AN AREA AND THE LOCAL MUNICIPALITIES. INTERRUPTION SHALL ONLY OCCUP.

ANY RECYCLED MATERIAL TO BE STOCKPILED ON THE SITE SHALL BE STEP ON THE SUILDING PAD AS A PARK A

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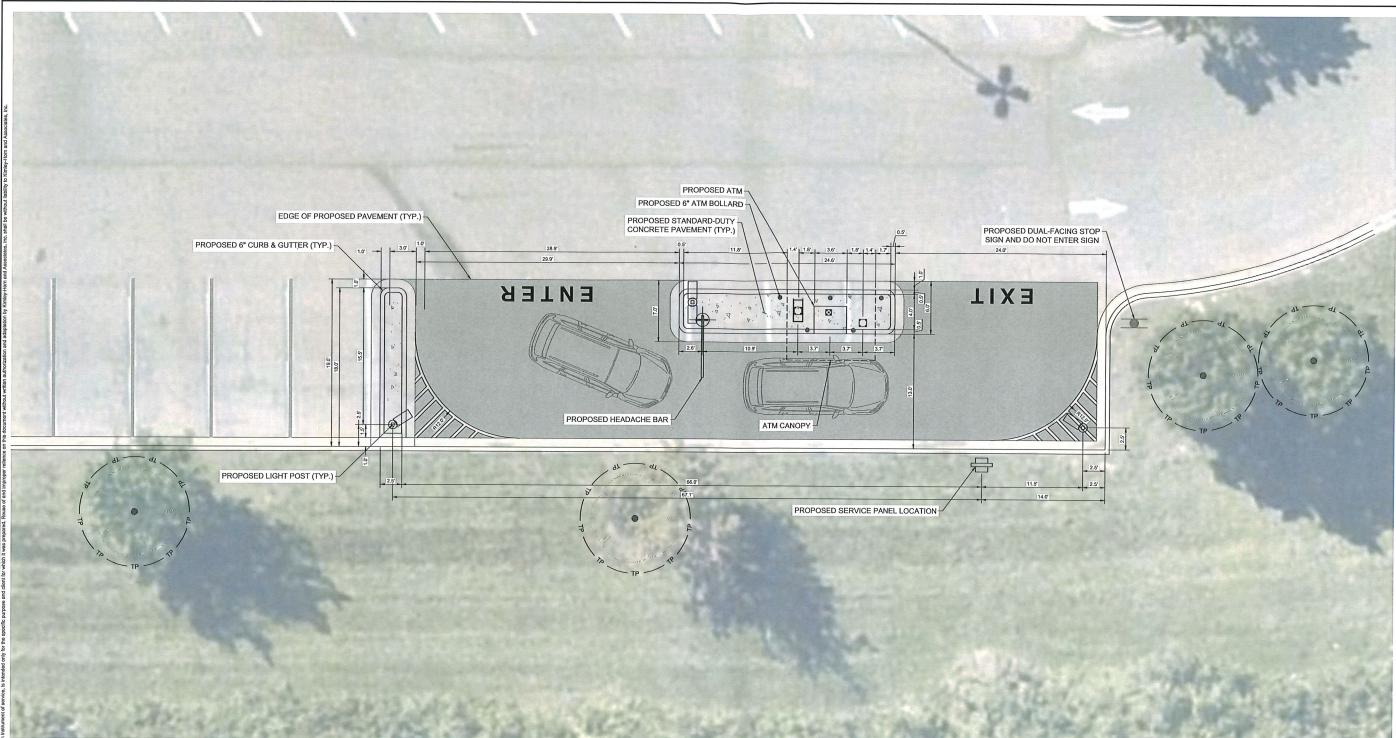
Kimley » Horn

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'ALMART #3502 ATM ' LITE BANKING CENT HAMILTON, OHIO

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NOTES

- DISTREED AREAS LEFT IDLE FOR FIVE DAYS, AND NOT TO FINAL GRADE, WILL BE ESTABLISHED TO TEMPORARY VEGETATION MULCH, TEMPORARY VEGETATION OR PERMANENT VEGETATION SHALL BE COMPLETED ON ALL EXPOSED AREAS WITHIN 14 DAYS AFTER DISTURBANCE, ALL AREAS TO FINAL GRADEWILL BE ESTABLISHED TO PERMANENT VEGETATION OR PERMANER. ALL AREAS TO FINAL GRADEWILL BE ESTABLISHED TO PERMANENT VEGETATION HON COMPLETION.

 WHEN HAND PLAITING, MULCH (HAY OR STRAW) SHOULD BE UNIFORMLY SPREAD OVER SEEDED AREA WITHIN 24 HOURS OF SEEDING, FUNABLE TO ACCOMPLISH, MULCH SHALL BE USED AS A TEMPORARY COVER, CONCENTRATED FLOW AREAS AND ALL SLOPES STEEPER THAY 2.51 AND WITH A HEIGHT OF THE PROPERTY OF THE PROPERTY OF THE METALLATION OF THE METALLATION OF EROSION AND SEDIMENT CONTROL METALLATION OF STRAIN OF THE METALLATION OF EROSION AND SEDIMENT CONTROL MESSURES AND PRACTICES PRIOR TO, OR CONCURRENT WITH, LAND-DISTURBING ACTIVITIES.

 SEED ALL DISTURBED AREAS UNLESS OTHERWISE NOTED AS PART OF THIS CONTRACT.

 THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXSTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE RESPONSIBLE FOR MY AND ADDISANCES WHICH MIGHT OF THE METALLATION OF EROSION INTO THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXSTING UTILITIES BEFORE COMMENCING MOKNA AND AGREES TO BE RESPONSIBLE FOR MY AND ALL DAMAGES WHICH MIGHT OF THE METALLATION OF THE METALLATION

- VIDE TEMPORARY CONSTRUCTION ACCESS(ES) AT THE POINT(S) WHERE CONSTRUCTION IICLES EXIT THE CONSTRUCTION AREA. MAINTAIN PUBLIC ROADWAYS FREE OF TRACKED MUD AND
- 10. DO NOT DISTURB VEGETATION OR REMOVE TREES EXCEPT WHEN NECESSARY FOR GRADING

DISCLAIMER

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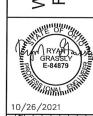
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WALMART #3502 ATM ADDITION FLITE BANKING CENTERS, LLC

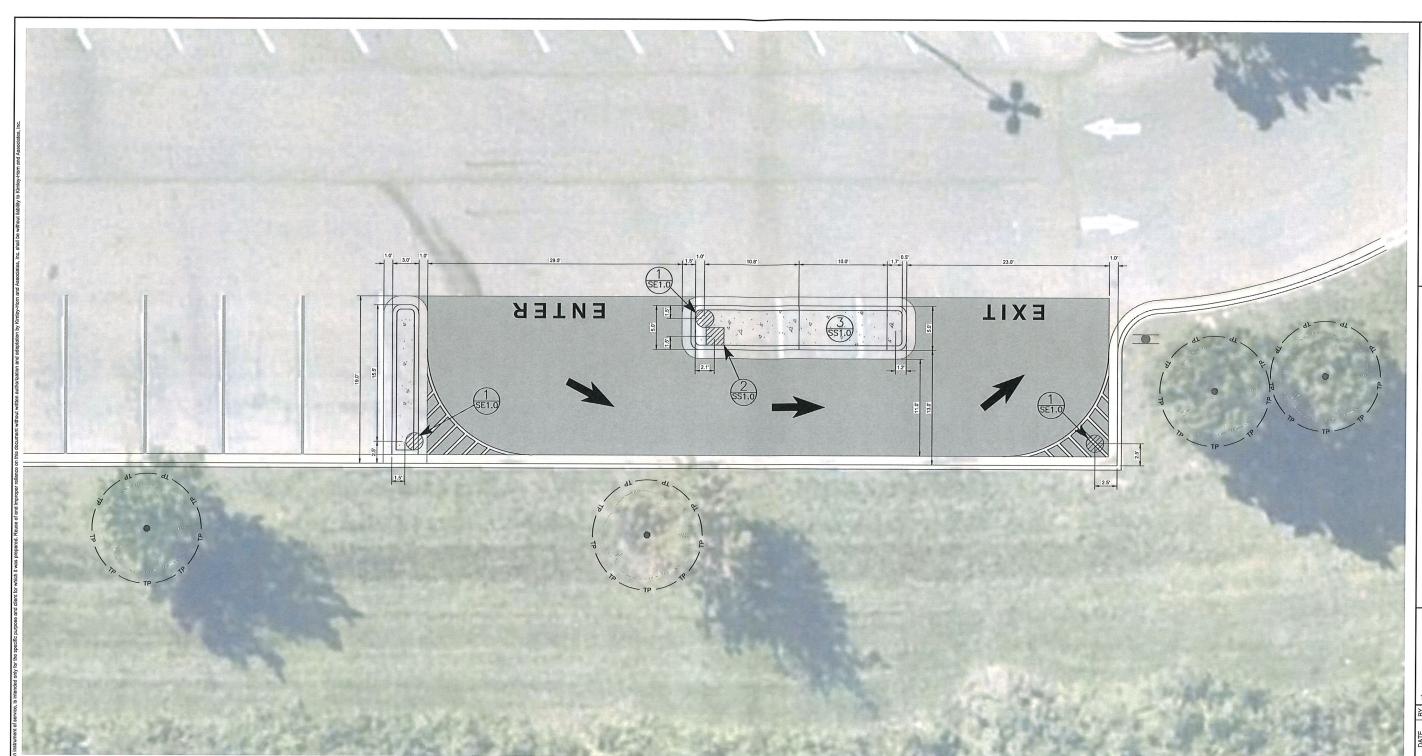


RAWN BY: HECKED BY:

KIMLEY-HORN PROJECT NO.

SITE PLAN

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NOTES

- STRIPING ON ALL CURBS, PAVEMENT, AND TRAFFIC ARROWS SHALL BE PAINTED WHITE. LETTERING 'ENTER' AND 'EXIT' SHALL BE PAINTED WHITE. CONTRACTOR SHALL MAINTAIN EXISTING DRAINAGE PATTERNS. CONTRACTOR SHALL ENSURE A SMOOTH TRANSITION TO EXISTING GRADES.

- DISTURBED AREAS LEFT IDLE FOR FIVE DAYS, AND NOT TO FINAL GRADE, WILL BE ESTABLISHED TO TEMPORARY VEGETATION, MULCH, TEMPORARY VEGETATION OR PERMANENT VEGETATION SHALL BE COMPLETED ON ALL EXPOSED AREAS WITHIN 14 DAYS AFTER DISTURBANCE, ALL AREAS TO FINAL GRADE WILL BE ESTABLISHED TO PERMANENT VEGETATION UPON COMPLETION. WHEN HAMD PLANTING, MULCH (HAY OR STRAWS HOLD BE UNIFORMLY SPREAD OVER SEEDED AREA WITHIN 24 HOURS OF SEEDING, IF UNABLE TO ACCOMPLISH, MULCH SHALL BE USED AS A TEMPORARY COVER CONCENTRATED FROM AREAS AND ALL SOPES STEEPER THAY 2.5.* THAN YEAR OWN THE APPROPRIATE EROSION CONTROL, MATTING OR BLANKETS, THE ESCAPE OF SEDIMENT FROM THE APPROPRIATE EROSION CONTROL, MATTING OR BLANKETS, THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO, OR CONCURRENT WITH.

- 10. DO NOT DISTURB VEGETATION OR REMOVE TREES EXCEPT WHEN NECESSARY FOR GRADING



LOCATION OF PROPOSED PIERS

DISCLAIMER

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PIER, PAINTING & STRIPING PLAN

KIMLEY-HORN PROJECT NO. 118420007

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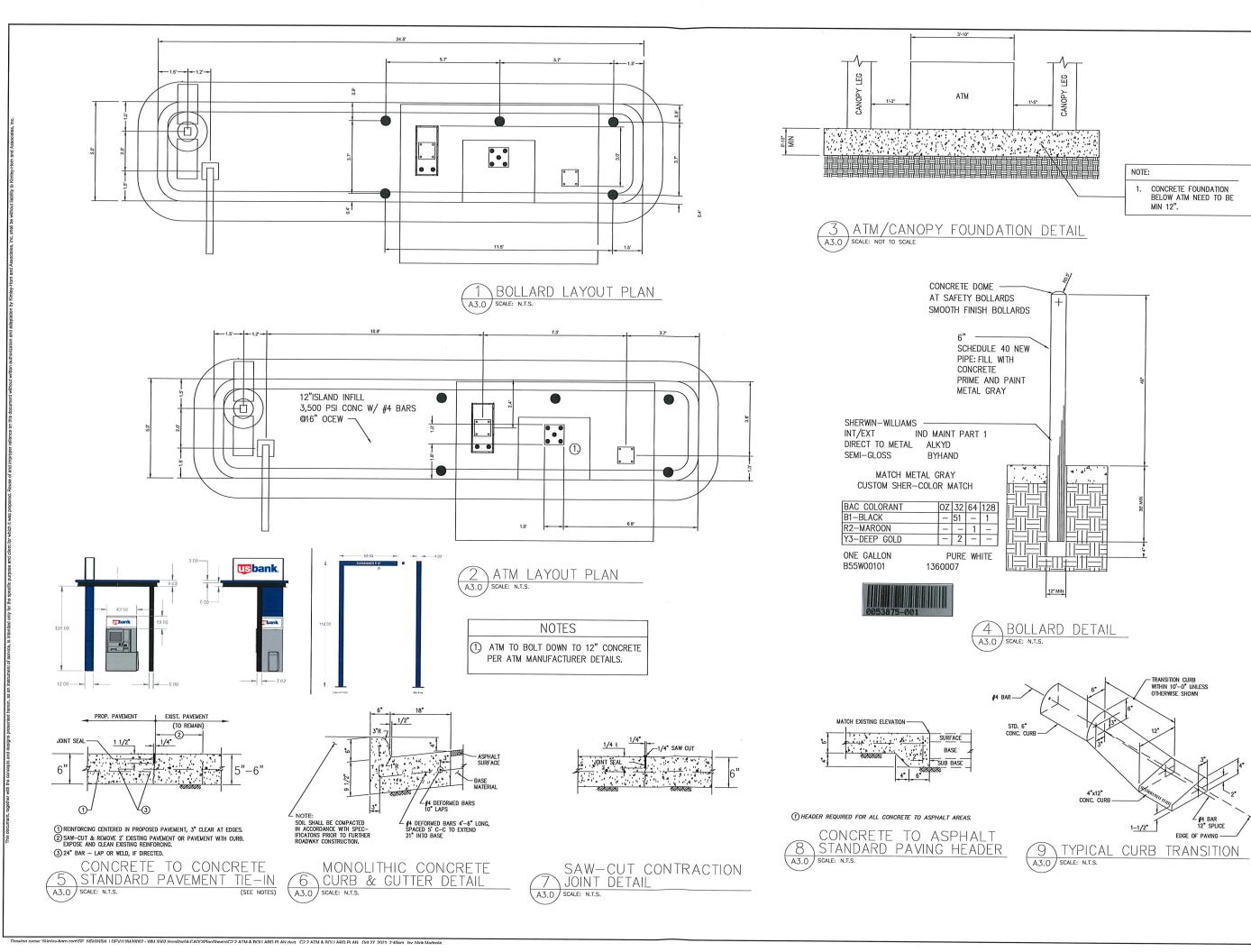
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CHECKED BY:

Kimley » Horn

WALMART #3502 ATM ADDITION FLITE BANKING CENTERS, LLC

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Kimley » Horn

WALMART #3502 ATM ADDITION FLITE BANKING CENTERS, LLC

10/26/2021

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ATM & BOLLARD PLAN

SHEET NUMBER C2.2