

ADDITION OF ATM DRIVE-UP IN EXISTING WALMART #3502 PARKING LOT
3201 PRINCETON ROAD
FAIRFIELD TOWNSHIP, OH

VICINITY MAP
NOT TO SCALE

PRINCETON RD

SITE

OH-4 BYPASS

BUTLER COUNTY VETERANS HWY

LOCATION MAP
NOT TO SCALE
©2011 NEARMAP

SHEET INDEX
SHEET TITLE
COVER
GENERAL NOTES
SITE LOCATION
EXISTING CONDITIONS & DEMOLITION PLAN
SITE PLAN
PIER, PAINTING & STRIPING PLAN
ATM & BOLLARD PLAN

Kimley»»Horn

PLAN REVISIONS		
REVISION NO.	DATE	REMARKS
1	10/26/2021	VARIANCE SUBMITTAL
FILE NUMBER	SHEET NUMBER	TOTAL SHEETS
118420007	C0.0	7



**Know what's below.
Call before you dig.**

Drawing name: \\kimley-horn.com\SE_NSH\NSH_LDEV\118420007 - WM 3502 Hamilton\4-CADD\PlanSheets\C0.0 COVER.dwg Oct 27, 2021 2:39pm by: Nick.Madonia

1. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL BE RESPONSIBLE FOR

- Drawing name: \\mimley-bom.com\SE_NSH\NSH_1\DEV118420007 - WJ 3502 Hamilton V-CADD\PlanSheets\C01 GENERAL NOTES.dwg C01 GENERAL NOTES Q=27 2021 2:39pm by Nick Marjonia

1. NOTIFICATIONS

THE CONTRACTOR SHALL NOTIFY THE OWNER AND CITY INSPECTOR(S) 24 HOURS PRIOR TO ANY DEMOLITION OR CONSTRUCTION.

2. DISPOSAL GUIDELINES:

- A. ONLY ITEMS SPECIFICALLY NOTED TO BE DEMOLISHED SHALL BE REMOVED FROM THE SITE.
- B. REMOVE EXISTING PAVED AREAS AS SHOWN INCLUDING DRIVEWAYS, SIDEWALKS, PARKING AREAS, SERVICE AREAS, EQUIPMENT PADS, AND ALL MISCELLANEOUS PAVING.
- C. ALL DEBRIS RESULTING FROM DEMOLITION SHALL BE REMOVED FROM THE SITE AND DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH LOCAL STATE AND FEDERAL REGULATIONS. BACKFILL ALL TRENCHES AND EXCAVATIONS RESULTING FROM DEMOLITION.
- D. ALL DEMOLISHED MATERIAL BECOMES THE PROPERTY OF THE CONTRACTOR UNLESS OTHERWISE NOTED.

3. TREE PROTECTION GUIDELINES

PROTECT ALL EXISTING TREES NOTED "TO REMAIN" AND ALL ITEMS TO BE TURNED OVER TO THE OWNER DURING DEMOLITION. TAKE ALL NECESSARY PRECAUTIONS AND PROTECTIVE MEASURES. ANY EXISTING ITEMS TO BE TURNED OVER TO THE OWNER WHICH ARE DAMAGED DURING DEMOLITION SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER. TREES WHICH ARE DAMAGED WILL BE REPLACED OR REIMBURSED AT A RATE TO BE DETERMINED BY THE OWNER.

4. UTILITIES:

- A. PRIOR TO REMOVING OR ABANDONING ANY UTILITY THE CONTRACTOR SHALL VERIFY THAT NO UPSTREAM SERVICE WILL BE TERMINATED. THE CONTRACTOR SHALL INFORM THE ENGINEER IN WRITING OF ANY TERMINATION NOT SHOWN ON THE PLANS.
- B. ALL ABANDONED WATER LINES, STORM SEWER PIPE, SANITARY SEWER PIPES, GAS LINES, OR ANY OTHER ABANDONED UNDERGROUND UTILITY SHALL BE ABANDONED IN PLACE UNLESS NOTED OTHERWISE.

1. THE FOLLOWING ARE APPLICABLE TO ALL CIVIL DOCUMENTS:

- A. WHERE A DETAIL SECTION, TYPICAL SECTION, OR A NOTE IS SHOWN FOR ONE CONDITION, IT SHALL APPLY FOR ALL LIKE OR SIMILAR CONDITIONS, UNLESS OTHERWISE NOTED ON THE PLANS.
- B. EXISTING AND PROPOSED CONTOURS ARE AT ONE (1) FOOT INTERVALS.
- C. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
- D. ALL PIPE LENGTHS SPECIFIED IN THESE PLANS ARE THE HORIZONTAL DISTANCE AND ARE SHOWN FOR REFERENCE ONLY. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE ACTUAL LENGTHS BASED ON PROPOSED PIPE SLOPE.
- E. PIPE LENGTHS IN PLANS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE UNLESS OTHERWISE NOTED.

1. COMPREHENSIVE

- A. THE ESCAPE OF SEDIMENT FROM THE SITE SHALL BE PREVENTED BY THE INSTALLATION OF EROSION AND SEDIMENT CONTROL MEASURES AND PRACTICES PRIOR TO OR CONCURRENT WITH LAND-DISTURBING ACTIVITIES.
- B. PROVISIONS TO PREVENT EROSION OF SOIL FROM THE SITE SHALL BE AT A MINIMUM IN CONFORMANCE WITH THE REQUIREMENTS OF THE OHIO EROSION AND SEDIMENT CONTROL HANDBOOKS. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
- C. FAILURE TO INSTALL, OPERATE, OR MAINTAIN ALL EROSION CONTROL MEASURES WILL RESULT IN ALL CONSTRUCTION BEING STOPPED ON THE JOB SITE UNTIL SUCH MEASURES ARE CORRECTED BACK TO THE STANDARDS SPECIFIED IN THE OHIO EROSION AND SEDIMENT CONTROL HANDBOOKS, CURRENT EDITION.
- D. EROSION CONTROL MEASURES SHALL BE MAINTAINED AT ALL TIMES. IF FULL IMPLEMENTATION OF THE APPROVED PLAN DOES NOT PROVIDE FOR EFFECTIVE EROSION CONTROL, ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IMPLEMENTED TO CONTROL OR TREAT THE SEDIMENT SOURCE.
- E. EROSION CONTROL DEVICES SHALL BE INSTALLED PRIOR TO LAND DISTURBANCE. THE LOCATION OF SOME OF THE EROSION CONTROL DEVICES MAY HAVE TO BE ALTERED FROM THAT SHOWN ON THE APPROVED PLANS IF DRAINAGE PATTERNS DURING CONSTRUCTION ARE DIFFERENT FROM THE FINAL PROPOSED DRAINAGE PATTERNS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ACCOMPLISH EROSION CONTROL FOR ALL DRAINAGE PATTERNS CREATED AT VARIOUS STAGES DURING CONSTRUCTION. ANY DIFFICULTY IN CONTROLLING EROSION DURING ANY PHASE OF CONSTRUCTION SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY.
- F. THE CONSTRUCTION OF THE SITE WILL COMMENCE WITH THE INSTALLATION OF EROSION CONTROL MEASURES SUFFICIENT TO CONTROL EROSION DEVICES AND EROSION. ALL SEDIMENT CONTROL WILL BE MAINTAINED UNTIL ALL UPSTREAM GROUND WITHIN THE CONSTRUCTION AREA HAS BEEN COMPLETELY STABILIZED WITH PERMANENT VEGETATION AND ALL ROADS/DRIVEWAYS HAVE BEEN PAVED.
- G. CONSTRUCTION EXITS SHALL BE CONSTRUCTED AT EACH POINT OF ENTRY OR EXIT FROM THE SITE AND SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH STONE AS CONDITIONS DEMAND, REPAIR, AND/OR CLEAN-UP OF ANY STRUCTURES USED TO TRAP SEDIMENT. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES OFF SITE ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY. ACCESS POINTS PROTECTED WITH A CONSTRUCTION EXIT SHALL BE OTHERWISE BARRICADED UNTIL THE SITE IS STABILIZED.

A. CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED BASED ON THE CITY STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS.

B. PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WARNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.

C. ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION.

D. ANY COMPONENTS OF THE PROJECT SERVING MULTIFAMILY DWELLINGS IN BUILDINGS THAT HAVE 4 OR MORE UNITS PER DWELLING SHALL ALSO CONFORM TO THE FAIR HOUSING ACT (FHA), AND COMPLY WITH THE FAIR HOUSING ACT DESIGN MANUAL BY THE US DEPARTMENT OF HOUSING AND URBAN DEVELOPMENT.

E. BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESS AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPE EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.

F. CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.

1. PRODUCT CRITERIA:

- A. UNLESS OTHERWISE NOTED CEMENT SHALL BE TYPE I OR III CONFORMING TO ASTM C150. AGGREGATES SHALL BE NORMAL WEIGHT CONFORMING TO ASTM C33.
 - B. CONCRETE SHALL CONFORM TO ACI BUILDING CODE (318-89). UNLESS NOTED CONCRETE SHALL BE NORMAL WEIGHT AND HAVE A 28 DAY COMPRESSIVE STRENGTH OF 3,500 P.S.I.
 - C. SLUMP SHALL BE 3" TO 5" FOR REGULAR MIX. LARGER SLUMP SHALL BE PERMITTED WITH WATER REDUCING ADMIXTURES AND WRITTEN CONSENT OF THE ENGINEER.
 - D. THE RELATIONSHIP BETWEEN MAXIMUM AGGREGATE SIZE TO MINIMUM AMOUNT OF CEMENT IN CONCRETE PAVEMENT (L.B. PER C.Y. OR MIX) SHALL BE AS FOLLOWS: 1" - 5/32" - 540, 1 1/2" - 590, 3/8" - 610.
2. CURING CRITERIA:
- A. CONCRETE CURING SHALL COMPLY WITH ACI 308. CURING PROCESS SHALL START IMMEDIATELY FOLLOWING INITIAL SET. CURING SHALL BE BY CURING COMPOUND.
 - B. CONCRETE EXPOSED TO THE WEATHER SHALL BE AIR-ENTRAINED IN ACCORDANCE WITH ACI 318-89 TABLE 4.1.1. NORMAL WEIGHT CONCRETE SLABS SHALL HAVE AIR CONTENT IN ACCORDANCE WITH ACI 302/IR-89 TABLE 5.2.7A.
 - C. HOT WEATHER CONCRETING SHALL COMPLY WITH ACI 305. NO CONCRETE ABOVE 90 DEGREES FAHRENHEIT SHALL BE POURED. LOWER CONCRETE TEMPERATURE BY COOLING WATER AND AGGREGATE. FORMS, STEEL, AND SUBGRADE SHALL BE SPRINKLED WITH COLD WATER. AFTER FINISHING CONCRETE USE LIGHT FOG SPRAY UNTIL CURING COMPOUND IS USED.
 - D. COLD WEATHER CONCRETING SHALL COMPLY WITH ACI 306. SPECIAL MATERIAL PROCEDURES SHALL BE PROVIDED DURING PLACING AND CURING OF CONCRETE BELOW 40 DEGREES FAHRENHEIT.
 - E. E. CURING, HOT, AND COLD WEATHER CONCRETING PROCEDURES ARE ONLY GIVEN AS A GUIDE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PREVENT CONCRETE DAMAGE AND CRACKS. DAMAGED OR CRACKED CONCRETE WILL NOT BE ACCEPTED.

1. PRODUCT CRITERIA:

- A. REINFORCING STEEL SHALL CONFORM TO ASTM A-615, GRADE 60 AND GRADE 40 FOR #3 AND SMALLER BARS. MINIMUM LAP 48" DIAMETER.
- B. WELDED WIRE FABRIC SHALL CONFORM TO ASTM A-185, MINIMUM LAP SHALL BE 8".
- C. REINFORCING THAT IS WELDED SHALL BE WELDABLE TYPE AND CONFORM TO ASTM A-706.

1. PAVEMENT:

- A. ALL MATERIALS, EQUIPMENT, METHODS OF CONSTRUCTION, AND WORKMANSHIP SHALL CONFORM TO THE OHIO DEPARTMENT OF TRANSPORTATION, ODOT, STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, CURRENT EDITION.
 - B. SEE PAVEMENT DETAILS ON CONSTRUCTION DOCUMENTS FOR SPECIFIC DESIGN INFORMATION AND REQUIREMENTS.
 - C. ALL CURB AND GUTTER TO BE 24" AND CONSTRUCTED OF 3000 P.S.I. CONCRETE UNLESS OTHERWISE NOTED.
2. SIGNING AND STRIPING:
- A. SIGNING AND STRIPING TO BE PROVIDED BY THE CONTRACTOR ACCORDING TO THE DRAWINGS AND SPECIFICATIONS.
 - B. ALL PAVEMENT MARKINGS SHALL CONFORM TO CURRENT MUTCD STANDARDS. ALL PAVEMENT MARKINGS ON PRIVATE PROPERTY SHALL BE PAINT, UNLESS NOTED OTHERWISE. ALL PAVEMENT MARKINGS ON PUBLIC RIGHT-OF-WAY SHALL BE THERMOPLASTIC, UNLESS NOTED OTHERWISE.

1. REINFORCED CONCRETE PIPE SHALL BE RCP CLASS III UNLESS OTHERWISE NOTED WITH BELL-AND-SPIGOT AND GASKETED JOINTS WITH ASTM C 443 RUBBER GASKETS.
2. FILL HEIGHTS OVER 13' REQUIRE CLASS IV RCP STORM PIPE.
3. STORM INLETS SHALL BE PRECAST IN ACCORDANCE WITH FAIRFIELD TOWNSHIP PUBLIC WORKS SPECIFICATIONS WHICH MEET OR EXCEED ASTM C 478.
4. ALL MANHOLE FRAMES AND COVERS ARE TO BE PER FAIRFIELD TOWNSHIP PUBLIC WORKS STANDARD DETAIL FOR DIMENSIONS AND MATERIALS AND AS BELOW IF NOT OTHERWISE INDICATED. FERROUS; 24-INCH (610-MM) ID BY 7- TO 9-INCH (175- TO 225-MM) RISER WITH 4-INCH (102-MM-) MINIMUM WIDTH FLANGE AND 26-INCH- (660-MM-) DIAMETER COVER. INCLUDE INDENTED TOP DESIGN WITH LETTERING CAST INTO COVER, USING WORDING EQUIVALENT TO "STORM SEWER."
5. MATERIAL: GRAY IRON ASTM A48 CLASS 30 UNLESS OTHERWISE INDICATED.
6. ALL HDPE PIPE SHALL BE CORRUGATED HIGH DENSITY POLYETHYLENE SMOOTH INTERIOR PIPE. HDPE PIPE SHALL CONFORM TO ASTM D3350 WITH SOIL TIGHT JOINTS.
7. ALL HDPE SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATIONS AND AASHTO SECTION 30.
8. CONTRACTOR TO PROVIDE AND INSTALL MANUFACTURER RECOMMENDED FITTINGS ON RCP CONNECTIONS TO HDPE STRUCTURES.
9. REFER TO PIPE CHART FOR CASTING TYPES. INSTALL REDUCERS AS NECESSARY PER MANUFACTURER'S SPECIFICATIONS TO ACCOMMODATE LARGER INLET SIZES.
10. FOLLOW CONSTRUCTION PLANS AND MANUFACTURER DETAILS, SPECIFICATIONS, AND INSTALLATION INSTRUCTION AS INCLUDED WITHIN THE PLANS AND PROVIDED BY MANUFACTURER FOR THE INSTALLATION OF WATER QUALITY AND DETENTION SYSTEMS.
11. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR ALL STORM SEWER PIPE, STRUCTURES, WATER QUALITY STRUCTURES, AND DETENTION STRUCTURES FOR ENGINEER AND OWNER APPROVAL PRIOR TO ORDERING MATERIALS.

- A. DO NOT EXCEED CLEARING AND GRUBBING LIMITS OF CONSTRUCTION LINES INDICATED ON THE PLANS.
- B. ALL AREAS OUTSIDE THE LIMITS OF CONSTRUCTION LINE SHALL NOT BE CROSSED BY HEAVY EQUIPMENT OR USED FOR STORING HEAVY EQUIPMENT OR MATERIALS.
- C. NO EQUIPMENT SHALL BE STORED UNDER THE DRIP LINE OF TREES TO REMAIN.
- D. DO NOT FALL ANY TREES OR PUSH PILES OF DEBRIS AGAINST ANY TREES TO REMAIN.
- E. REMOVE ALL STUMPS, ROCKS, ASPHALT & CONCRETE DEBRIS, ETC. AND DISPOSE OFF SITE IN ACCORDANCE WITH LOCAL, STATE & FEDERAL REGULATIONS.
- F. CONTACT ALL UTILITY AUTHORITIES WHO HAVE LINES WITHIN THE CLEARING AND GRUBBING LIMITS BEFORE STARTING WORK
- E. ALL EROSION CONTROL SEDIMENT BARRIERS, SILT FENCES, AND TREE PROTECTION DEVICES SHALL BE INSTALLED PRIOR TO STARTING CLEARING AND GRUBBING.
- F. AFTER STAKING IS COMPLETED, TREES WITHIN GRADING LIMITS TO BE SAVED WILL BE IDENTIFIED BY THE OWNER'S REPRESENTATIVE. FIELD CHANGES TO GRADING PLANS SHALL BE MADE FOR SMOOTH TRANSITION OF GRADES AROUND ALL TREES WHICH REQUIRE TREE WELLS WITHIN THE GRADING LIMITS.
- G. ALL CLEARING SHALL BE LIMITED TO AREAS TO BE GRADED WITHIN 15 CALENDAR DAYS.

TOPSOIL SHALL BE STORED ON SITE IN LOCATIONS APPROVED BY THE OWNER'S REPRESENTATIVE. DRAINAGE SHALL ROUT AROUND THESE TOPSOIL STOCKPILES FOR THE DURATION OF THE GRADING OPERATIONS. EROSION CONTROL MEASURES SHALL PREVENT THE LOSS OF TOPSOIL MATERIAL.

UNSUITABLE SOILS SHALL BE UNIFORMLY SPREAD ACROSS NON-STRUCTURAL FILL AREAS AND COVERED WITH TOPSOIL AND SEEDED.

FILL AREA SHALL BE PROOF-ROLLED WITH RUBBER-TIRED EQUIPMENT WITH A MINIMUM WEIGHT OF FIFTEEN TONS PRIOR TO BEGINNING FILL OPERATION. AREAS WHICH ARE SOFT OR UNSTABLE SHALL BE UNDERCUT UNTIL STABLE SOILS ARE FOUND. RE-COMPACTION OF THESE SOILS SHALL BE TO 98% MAXIMUM DRY DENSITY AS PER ASTM D698 (STANDARD PROCTOR).

CUT AREA SHALL BE PROOF-ROLLED AFTER FINAL SUBGRADE IS ACHIEVED IN THE SAME MANNER AS FILLED AREAS. SOFT OR UNSTABLE SOILS SHALL BE SCARIFIED TO A DEPTH OF 12" AND RE-COMPACTED TO 98% MAXIMUM DRY DENSITY AS PER ASTM D698 (STANDARD PROCTOR).

ALL GRADING SHALL BE COMPLETED TO THE LEVEL INDICATED BY THE SCOPE OF WORK LISTED IN THE BID DOCUMENTS.

ELEVATIONS SHOWN ON THE PLANS IS THE FINISH GRADE ELEVATION.

GRADING SHALL BE SEQUENCED SO THAT BASE STONE IS PLACED WITHIN 10 CALENDAR DAYS PF ACHIEVING OPTIMUM SUBGRADE COMPACTION.

SOILS TESTING LABORATORY/ SOILS ENGINEER, CONTRACTOR WILL EMPLOY A QUALIFIED SOILS TESTING LABORATORY/ ENGINEER TO OBSERVE THIS WORK AND MAKE TESTS AS REQUIRED.

HAVE EARTH BORROW FILL, AGGREGATE, AND TOPSOIL, AND STRUCTURAL FILL TESTED AND APPROVED BY DESIGNATED TESTING LABORATORY BEFORE MOVING IT TO THE JOB SITE.

CONTRACTOR SHALL OBSERVE PROOF-ROLLING OF AREAS WHERE BUILDING AND PAVING WILL BE LOCATED TO DETERMINE ADEQUACY OF SOIL COMPACTION AND IN-PLACE SOILS. OTHER AREAS WILL BE INSPECTED BY SOILS ENGINEER TO DETERMINE ADEQUACY IN THOSE AREAS. IF SOILS ARE NOT ADEQUATE TO BEAR WEIGHTS THAT WILL BE IMPOSED, TESTING LABORATORY WILL OBSERVE AND REPORT CORRECTIVE ACTION TAKEN.

TEST IN-PLACE SOIL AND FILLED AND COMPACTED AREAS. IF THESE ARE NOT ADEQUATE TO BEAR WEIGHTS IMPOSED, TESTING LABORATORY WILL ADVISE THE OWNER'S REPRESENTATIVE OF THEIR RECOMMENDATIONS. HE WILL DIRECT ANY CORRECTIVE MEASURES THAT ARE NECESSARY.

SOILS COMPACTION TESTING OF IN-PLACE AND FILLED AND COMPACTED AREAS WILL BE PERFORMED BY TESTING LABORATORY IN ACCORDANCE WITH THEIR REQUIREMENTS.

THE SOILS ENGINEER'S AND TESTING LABORATORY'S FEES WILL BE PAID BY THE CONTRACTOR.

APPLICABLE SPECIFICATIONS FOR COMPACTED FILL: THE FOLLOWING CURRENT AMERICAN SOCIETY OF TESTING MATERIALS (ASTM) STANDARDS ARE HEREBY MADE PART OF THIS SPECIFICATION:

- D421-58, DRY PREPARATION OF SOIL SAMPLES FOR GRAIN-SIZE ANALYSIS AND DETERMINATION OF SOIL CONSTANTS.
- D422-63, STANDARD METHOD OF PARTICLE SIZE ANALYSIS OF SOILS.
- D1140-54, METHOD OF TEST FOR AMOUNT OF MATERIAL IN SOILS FINER THAN NO.200 SIEVE.
- D698, METHOD FOR LABORATORY COMPACTION CHARACTERISTICS OF SOIL USING STANDARD EFFORT
- D1557-78, STANDARD TEST METHODS FOR MOISTURE-DENSITY RELATIONS OF SOILS AND SOIL-AGGREGATE MIXTURES USING 10 LB. (4.54-KG) RAMMER AND 18-INCH (457 MM) DROP.

PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDINGS AT ALL LOCATIONS UNLESS OTHERWISE NOTED.

CONTRACTOR SHALL REVIEW THE SITE SPECIFIC GEOTECHNICAL REPORT PRIOR TO COMMENCING WITH GRADING OPERATIONS. WHERE CONFLICTS BETWEEN THE GRADING NOTES AND GEOTECHNICAL REPORT EXIST, THE MORE STRINGENT REQUIREMENT SHALL APPLY.

CONTRACTOR IS RESPONSIBLE FOR DOCUMENTING WITH LOAD TICKETS, PHOTOGRAPHS, LOG BOOK, VIDEO RECORDING AND OTHER MEANS AS NECESSARY TO VERIFY THE INSTALLATION OF STORMWATER BEST MANAGEMENT PRACTICES REQUIRED BY THE LOCAL MUNICIPALITY AND JURISDICTION. AS-BUILT TOPOGRAPHY AND UTILITY PLANS HAVING BEEN PREPARED BY A QUALIFIED LAND SURVEYOR ARE REQUIRED TO BE SUBMITTED TO KIMLEY-HORN AT THE CONCLUSION OF THE PROJECT FOR VERIFICATION OF DESIGN INTENT. ANY MODIFICATIONS TO THE GRADING AND UTILITY SYSTEMS REQUIRED, NOT PREVIOUSLY APPROVED BY THE OWNER AND ENGINEER ARE THE RESPONSIBILITY OF THE CONTRACTOR.

WALMART #3502 ATM ADDITION
FLITE BANKING CENTERS, LLC
HAMILTON, OHIO



10/26/2021

NO	KEYS/ISS	DATE	BT
1	VARIANCE SUBMITTAL	10/26/2021	NMM
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DESIGNED BY:

AN BY:

BOOKED BY:

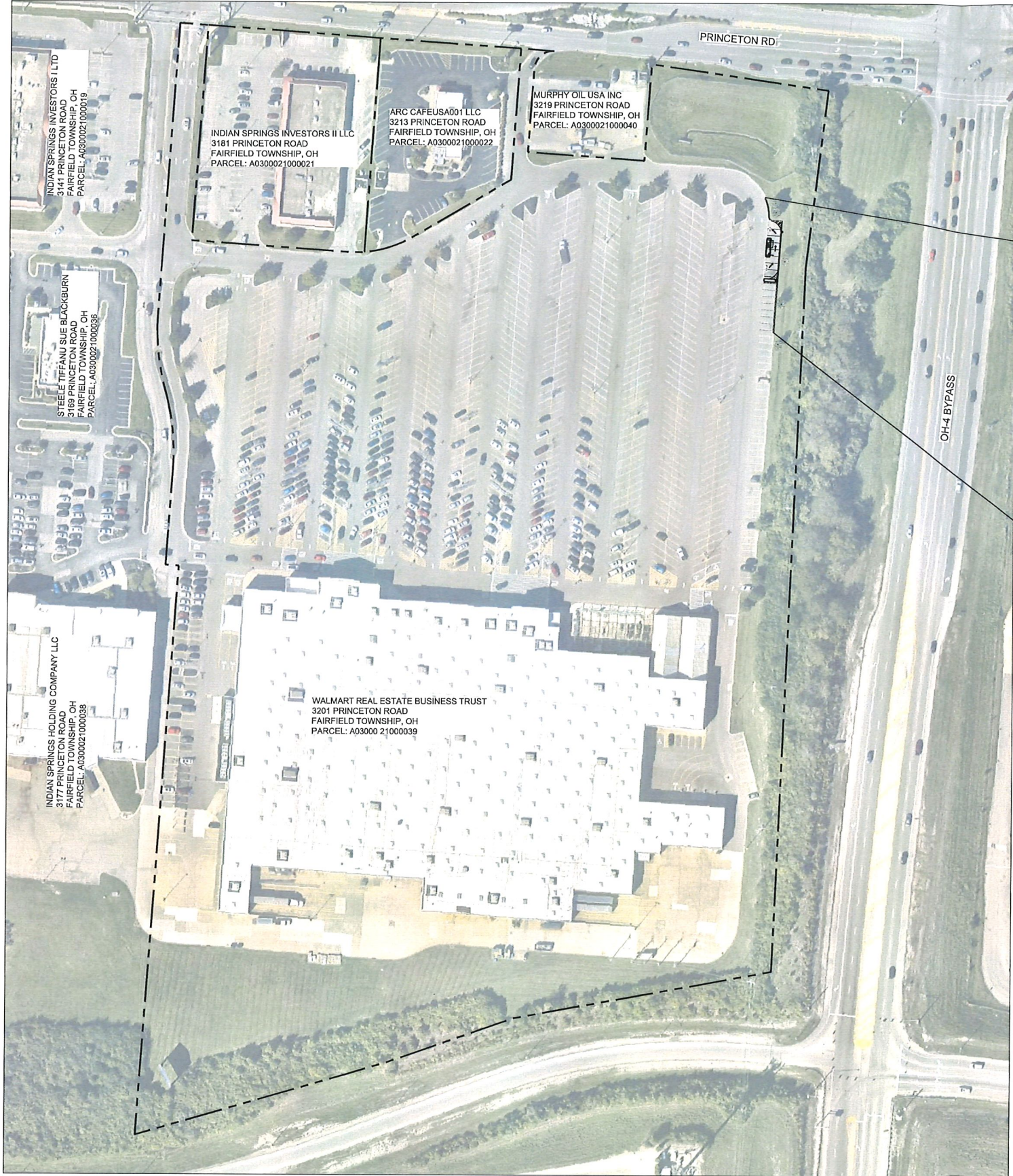
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GENERAL NOTES

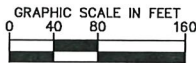
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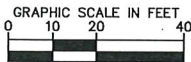
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OVERALL VIEW



ENLARGED VIEW



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CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL FIELD CONDITIONS AND IS TO ALERT THE ENGINEER AND FLITE BANKING CENTERS OF ANY DISCREPANCIES PRIOR TO STARTING CONSTRUCTION. CONTRACTOR TO COORDINATE WITH FLITE BANKING CENTERS PM FOR ALL FINAL PLACEMENTS OF INFRASTRUCTURE.

EXISTING PROPERTY SUMMARY

PROPERTY ADDRESS: 3201 PRINCETON ROAD
FAIRFIELD TOWNSHIP, OH
PLAT: N/A
LOT: 02
SUBD: N/A
MAP AND PARCEL ID: MAP A03, PARCEL 21000039
U.S. FEMA FIRM PANEL: 39017C0194E DATED 12/17/2010
EXISTING ZONING: M-1 - LIGHT INDUSTRIAL DISTRICT
EXISTING LAND USE: COMMERCIAL

Kimley»Horn

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**WALMART #3502 ATM ADDITION
FLITE BANKING CENTERS, LLC**
HAMILTON, OHIO

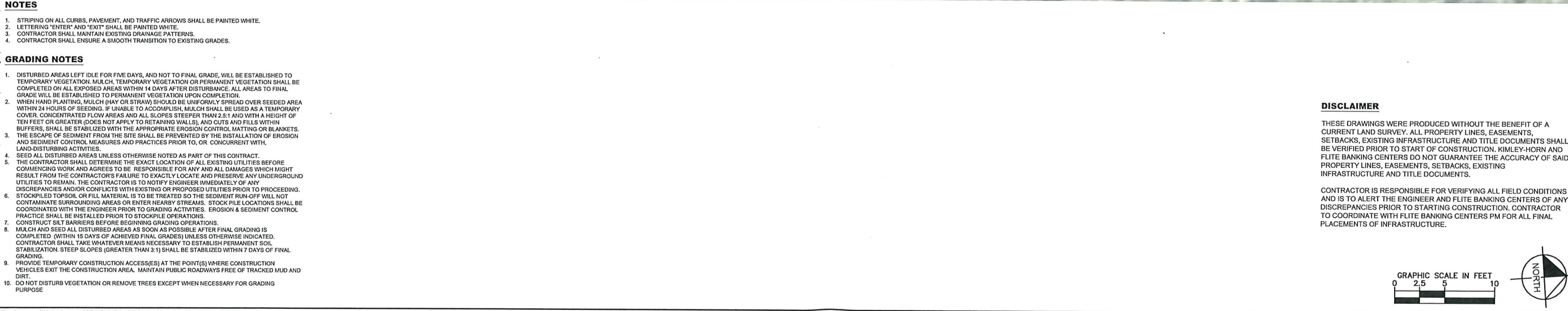


10/26/2021

REVISIONS	DATE	BY
VARIANCE SUBMITTAL	10/26/2021	NMM
No.	1	2
DESIGNED BY:	NMM	
DRAWN BY:	TAJ	
CHECKED BY:	RBG	
DATE:	10/26/2021	
KIMLEY-HORN PROJECT NO.	118420007	
SITE LOCATION		
SHEET NUMBER	C1.0	

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C1.1



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WALMART #3502 ATM ADDITION
FLITE BANKING CENTERS, LLC
HAMILTON, OHIO



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VARIANCE SUBMITTAL		10/26/2021	NMM
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DESIGNED BY:	2		
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KIMLEY-HORN PROJECT NO.
118420007

SITE PLAN

SHEET NUMBER

C2.0

TJ

RBG

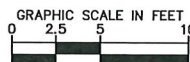
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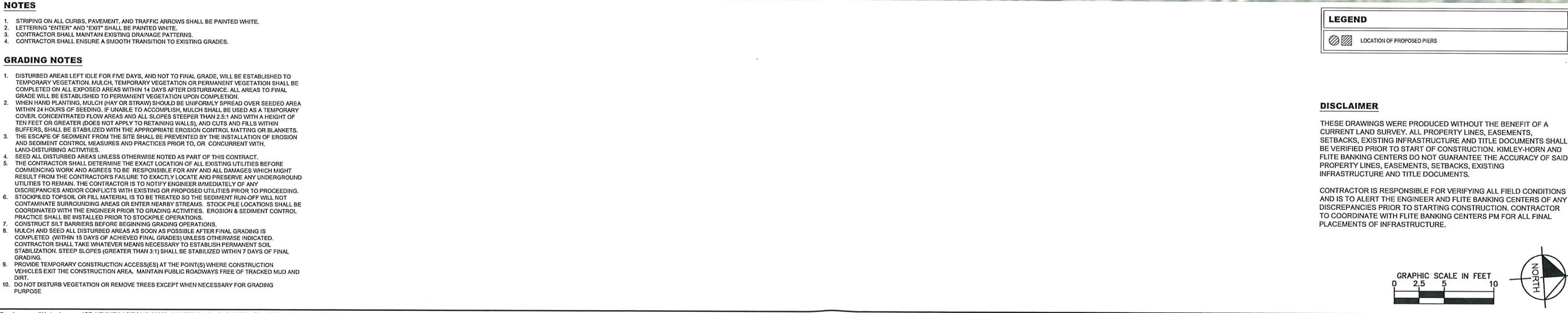
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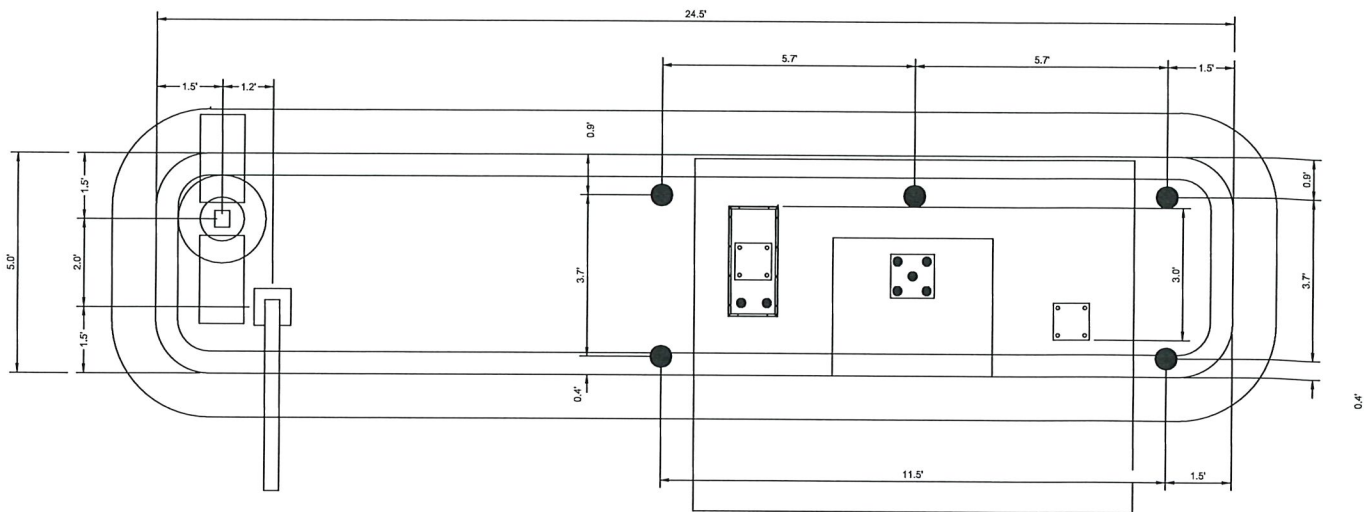
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FLITE BANKING CENTERS, LLC
HAMILTON, OHIO



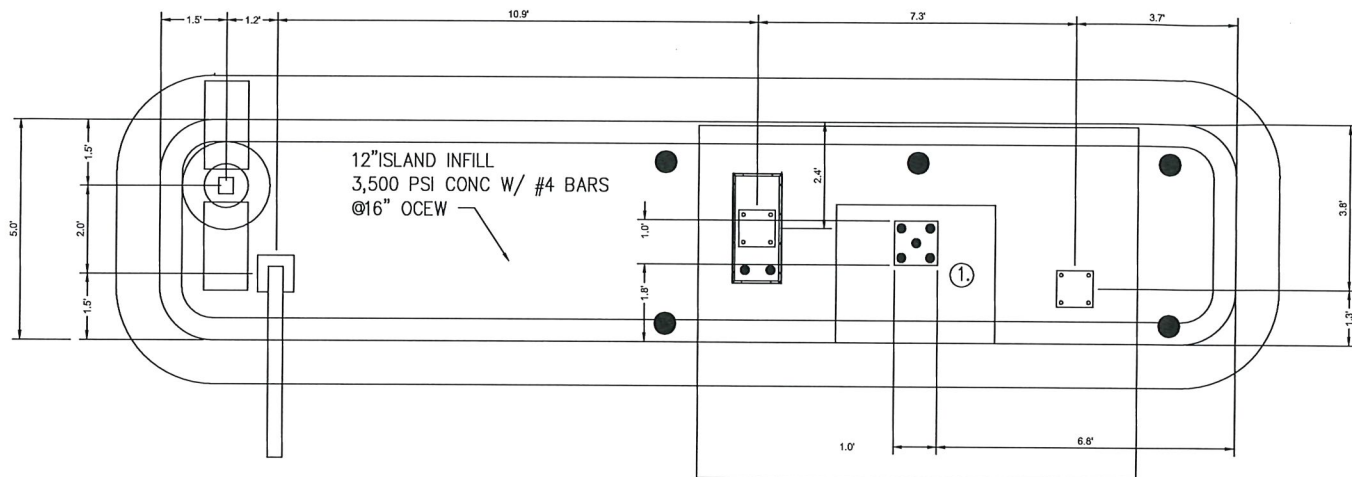
10/26/2021

NO.		DESIGNED BY:		NM	
DRAWN BY:		TA			
CHECKED BY:		RB			
DATE:		10/26/202			
KIMLEY-HORN PROJECT NO.		118420007			
PIER, PAINTING & STRIPING PLAN					
SHEET NUMBER		C2.1			

This document, together with the concepts and designs presented herein, is an instrument of service, is intended only for the specific purpose and client for which it was prepared, and is not to be used for any other purpose without the written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

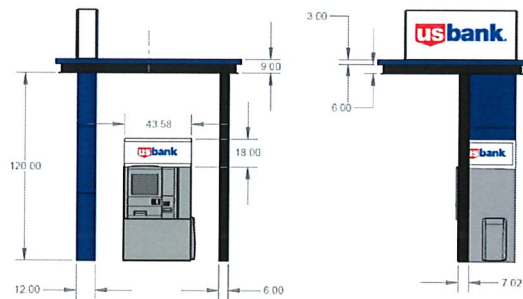


1 BOLLARD LAYOUT PLAN
A3.0 SCALE: N.T.S.

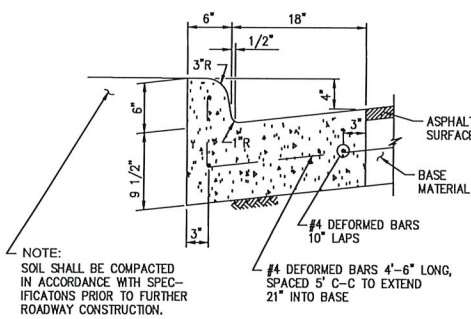


2 ATM LAYOUT PLAN
A3.0 SCALE: N.T.S.

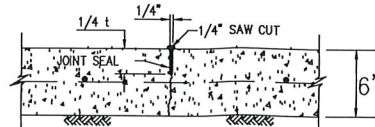
NOTES
① ATM TO BOLT DOWN TO 12" CONCRETE PER ATM MANUFACTURER DETAILS.



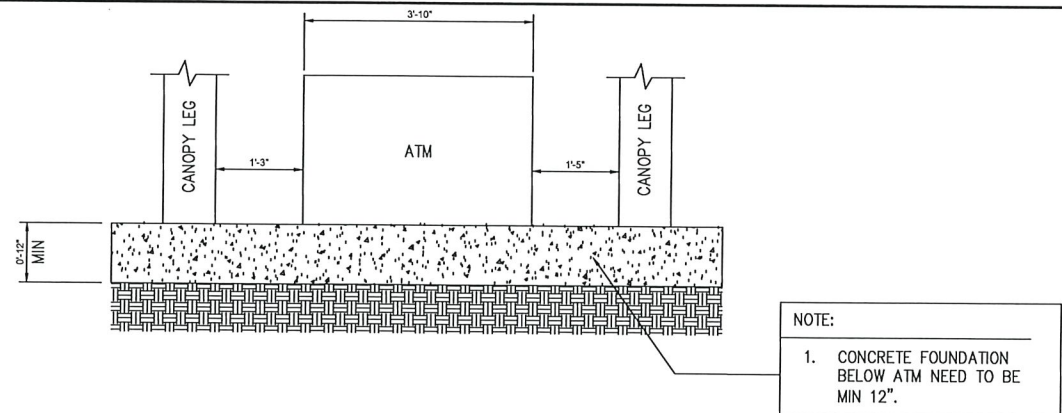
5 CONCRETE TO CONCRETE STANDARD PAVEMENT TIE-IN
A3.0 SCALE: N.T.S. (SEE NOTES)



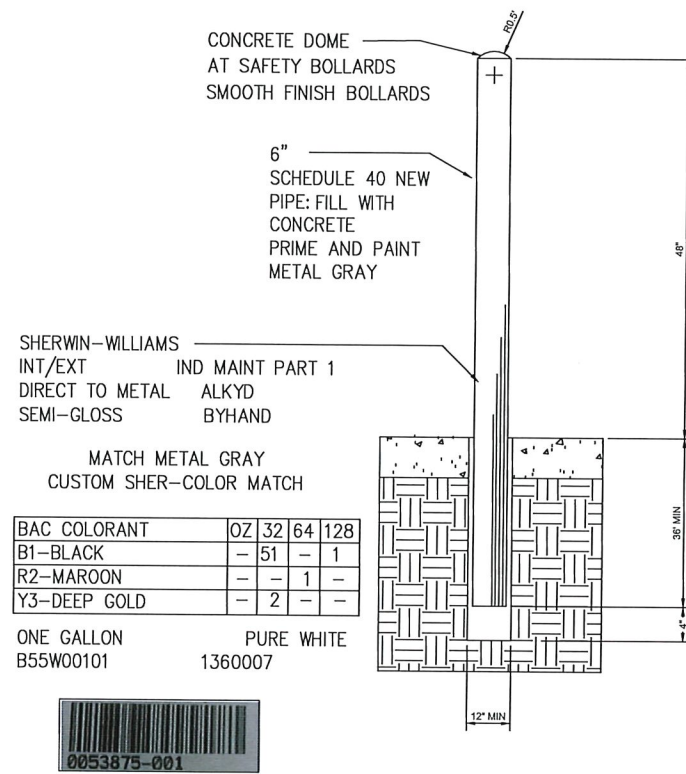
6 MONOLITHIC CONCRETE CURB & GUTTER DETAIL
A3.0 SCALE: N.T.S.



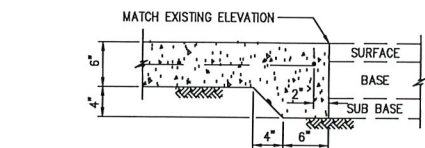
7 SAW-CUT CONTRACTION JOINT DETAIL
A3.0 SCALE: N.T.S.



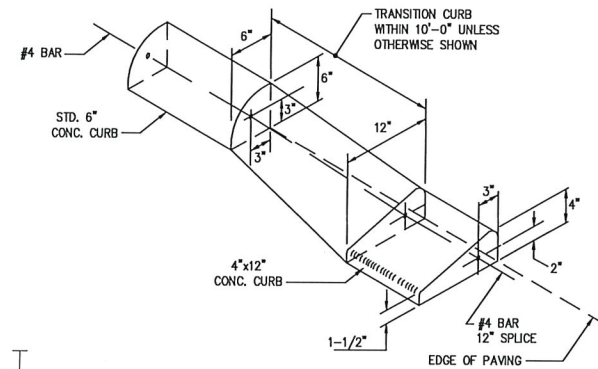
3 ATM/CANOPY FOUNDATION DETAIL
A3.0 SCALE: NOT TO SCALE



4 BOLLARD DETAIL
A3.0 SCALE: N.T.S.



8 CONCRETE TO ASPHALT STANDARD PAVING HEADER
A3.0 SCALE: N.T.S.



9 TYPICAL CURB TRANSITION
A3.0 SCALE: N.T.S.

Kimley»Horn

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WALMART #3502 ATM ADDITION
FLITE BANKING CENTERS, LLC
HAMILTON, OHIO



10/26/2021

NO.	DATE	BY	REVISIONS
1	10/26/2021	NMM	VARIANCE SUBMITTAL
2			
3			
4			
5			
6			
7			
8			
9			
10			

ATM & BOLLARD PLAN

SHEET NUMBER
C2.2